



**OFFICIAL
COMMUNITY
PLAN**

ASHCROFT



2018

Bylaw No. 822, 2018

WHEREAS the Council of the Village of Ashcroft wishes to repeal “Village of Ashcroft Official Community Plan Bylaw No. 737, 2005”, and all amendments thereto, and wishes to adopt a new Official Community Plan pursuant to Part 14, Division 4 of the *Local Government Act*.

AND WHEREAS a Public Hearing has been held pursuant to Part 14, Division 3 of the *Local Government Act*.

THEREFORE, BE IT RESOLVED that the Council of the Village of Ashcroft, in open meeting assembled, enacts as follows:

1. This bylaw will be cited as the “Village of Ashcroft Official Community Plan Bylaw No. 822, 2018”.
2. The following schedules attached hereto are hereby made part of this bylaw and adopted as the Village of Ashcroft Official Community Plan Bylaw:
 - a) Schedule A (Official Community Plan Text)
 - b) Schedule B, C, D, E, and F (Official Community Plan Maps)
 - c) Schedule G (Downtown Façade Design Guidelines)
3. If any section, subsection, sentence, clause or phrase of this bylaw is for any reason held to be invalid by the decision of any court of competent jurisdiction, the invalid portion shall be reversed and the decision that it is invalid shall not affect the validity of the remainder.
4. “Village of Ashcroft Official Community Plan Bylaw No. 737, 2005” and all amendments thereto are hereby repealed.

Read a first time this 23rd day of July, 2018

Read a second time this 24th day of September, 2018

Public hearing held on this 22nd day of October, 2018

Read a third time this 22nd day of October, 2018

Adopted this 22nd day of October, 2018

Original Signed by Mayor Jeyes

Mayor

Original Signed by CAO Allen

Chief Administrative Officer

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Schedule A - Official Community Plan Text

Schedule B, C, D, E, and F - Official Community Plan Maps

Schedule G – Downtown Façade Design Guidelines



PART
1

**Introduction
& Vision**

1. Introduction

1.1. Background

The Village of Ashcroft was incorporated in 1952, and, according to BC Stats' estimate for 2017, has 1,531 residents. The community is located 90 km west of Kamloops and is bisected by the Thompson River, which divides the downtown on the south side from residential areas and services on the north side.



Ashcroft is also where the Bonaparte River and Thompson River meet and is within the traditional territory of the Nlaka'pamux and Secwepemc First Nations.

Ashcroft has existed since the 1850s, filling a role as a transportation hub and as an agricultural producer. The village is known for its historic character and has been a popular site for filming movies and TV series. Striking features of the community are the Canadian Pacific and Canadian National railways which border the Thompson River, the river itself, and the sagebrush landscape surrounding the community. There is a significant industrial presence both on the north and south side of the Thompson River, and a transloading facility northeast of the community within the municipal boundary. The residential nature of the village is a mix of urban and suburban, with a mixed commercial/residential area in the downtown and areas with single family and multiple family homes.

Ashcroft is a community in transition with an aging population, shifting economy, and changing roles of government. Recognizing this transition, the Village has developed this Official Community Plan to prepare itself for the challenges and opportunities that Ashcroft will encounter in the future.

1.2. What is an Official Community Plan?

An Official Community Plan (OCP) is a policy document used by local governments to guide land use decisions and to establish policies and goals regarding community development. An OCP is a general statement of the collective desires of a community, identified through public consultation and expressed through broad goals and policy statements.

An OCP must be prepared and adopted within the statutory provisions of the *Local Government Act* (the Act). The required content of an OCP is defined in Section 473 of the Act. Required content includes:

- .1 Statements and map designations for the area covered by the plan respecting the following:
 - a) the approximate location, amount, type and density of residential development required to meet anticipated housing needs over a period of at least 5 years;

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- b) the approximate location, amount and type of present and proposed commercial, industrial, institutional, agricultural, recreational and public utility land uses;
 - c) the approximate location and area of sand and gravel deposits that are suitable for future sand and gravel extraction;
 - d) restrictions on the use of land that is subject to hazardous conditions or that is environmentally sensitive to development;
 - e) the approximate location and phasing of any major road, sewer and water systems;
 - f) the approximate location and type of present and proposed public facilities, including schools, parks and waste treatment and disposal sites; and
 - g) other matters that may, in respect of any plan, be required or authorized by the minister.
- .2 Housing policies of the local government respecting affordable housing, rental housing, and special needs housing.
- .3 Targets for the reduction of greenhouse gas emissions in the area covered by the plan, and policies and actions of the local government proposed with respect to achieving those targets.
- .4 In developing an official community plan, the local government must consider any applicable guidelines under Section 582 [provincial policy guidelines].

Optional content provisions are set out in Section 474 of the *Local Government Act* and may include:

- .1 Policies of the local government relating to social needs, social well-being, and social development;
- .2 A regional context statement, consistent with the rest of the plan, of how matters referred to in section 429 (2) (a) to (d) [content of regional growth strategy], and other matters dealt with in the plan, apply in a regional context;
- .3 Policies of the local government respecting the maintenance and enhancement of farming on land in a farming area or in an area designated for agricultural use in the plan; and
- .4 Policies of the local government relating to the preservation, protection, restoration and enhancement of the natural environment, its ecosystems, and biological diversity.

A local government may also state broad objectives in an official community plan with respect to matters that are outside of its jurisdiction.

An OCP is a long-term visionary document that guides decision-making and sets a course for how a community wishes to evolve. An OCP should inform decision making on a wide range of municipal planning and development functions. While an OCP should be a visionary, inspiring document, it also must be practical so that the community is able to achieve what it sets out to do in its OCP. An OCP should be re-examined and updated every 5 – 10 years to ensure that it continues to reflect the long-range planning goals of the community. Occasional updates also ensure that an OCP remains consistent with other procedures, bylaws and government implementation tools.

Before it can come into effect, an OCP must be adopted by Council as an official bylaw. Once adopted, all bylaws enacted or works undertaken by Council must be consistent with the goals and policies included in the Plan.

An Official Community Plan serves a number of important purposes:

- It provides direction for the growth and development of the community;
- It states in general terms the social, economic, and environmental objectives of a community as they relate to land use planning;
- It provides a tangible policy tool which Council, Village staff, and the general public can use to evaluate development proposals; and
- It establishes policy statements for land use and community development goals.

What an OCP Can and Cannot Do

An OCP Can:

- Provide a degree of certainty to Council, government staff, local businesses, developers, and residents regarding the future form and character of a community;
- Limit decisions and actions by Council when adopting bylaws or undertaking works to those which are consistent with the goals, objectives and policies included in the OCP;
- Provide direction for Council on decision-making and policy development; and
- Be used to support applications to acquire funding from senior levels of government to provide for works and services in a community, as funding is often contingent on a plan being in place for capital-intensive infrastructure investments.

An OCP Cannot:

- Obligate Council to undertake any projects suggested in the OCP;
- Commit Council to specific expenditures; or
- Force any action identified in the OCP.

An Official Community Plan

should be exactly what the name suggests:

- **Official**, meaning it is adopted by the Mayor and Council.
- **Community**, meaning it should reflect the community's desires and vision for the future along with implying that the community has a strong role in ensuring the plan is implemented.
- **Plan**, meaning there is a relevant strategy for achieving future goals and vision.

1.3. Why was the Ashcroft OCP updated?

There are several reasons why it was timely for the Village to update the OCP. Some of these reasons include:

- **Age of the Current OCP** – best practices suggest that OCPs should be reviewed and updated every 5 to 10 years. The current OCP was adopted in 2005 and is 13 years old. Also, the previous OCP was very much focussed on land use while many of the key community issues that need to be addressed are not wholly land use oriented. In this sense, it is important that the OCP is structured to reflect solutions to current and foreseen challenges.
- **Ashcroft is in Transition** - since 2005 the population of Ashcroft has decreased by 106 people, or by 6%. The median age of the population has also increased. During this time Ashcroft Elementary was closed and Desert Sands Community School was established as a K-12 school to serve the area. There has been the gradual emergence of a strong arts community that needs to be reflected. The development of Ashcroft Terminal could have significant impacts on the community as well.
- **There are Emerging Trends in Community Development that Need to be Discussed** – the sharing economy (i.e. Air BnB), new approaches to community food security, secondary suites, and alternative energy have emerged since the previous OCP was adopted and could impact how Ashcroft evolves in the future.
- **Community Services Have Changed** – the community is currently confronting a number of challenges related to the services provided in Ashcroft, particularly relating to healthcare and wellness. Leveraging the OCP development process to discuss these issues further is critical to finding solutions that will support the long-term sustainability of the community.
- **The Need to Enhance the Village’s Relationship with First Nations** – it is important that the Village use the OCP as another means of developing stronger relationships with local First Nations communities.
- **Support Economic Development** - a comprehensive and up-to-date OCP can provide certainty for developers that may be interested in undertaking a major project in the area. It will provide guidance to the Village of Ashcroft, the community, businesses, and senior government agencies on how to balance various economic development sectors to ensure long-term viability.
- **OCPs Can Help Support Infrastructure Funding** – senior governments often provide municipalities with funding through competitive grants to update and/or develop new infrastructure. Having a recently adopted OCP in place that provides guidance and policy direction on municipal infrastructure is important to support these funding grant applications.

1.4. Scope of the Plan and Approach

The approach to this OCP is to include all relevant goals and policies that will support the community in advancing towards common goals and the community vision for the future. While it is important for the OCP to be grounded in actions that the municipality has immediate jurisdiction over, it is also important to recognize that the OCP can also be used to communicate to the community at-large, senior government agencies, and industries the vision, goals and policies that the community supports. Therefore, many objectives and policy statements that are described in this OCP, specifically in regard to community development, are more closely aligned with community-led actions than administrative or Council-led actions.

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In terms of determining what goals, policies, and actions Ashcroft should pursue, significant community engagement and background research was undertaken. The following stages and actions constitute the community engagement carried out between 2016 and 2018 to support the creation of this OCP:

Pre-Engagement Period – this period consisted of a resident survey, which was distributed online and via mail to all residents of Ashcroft. A total of 180 responses were returned during this period, of which 99 were complete.

Engagement Week – from November 27 – 30, 2017, residents were engaged on a number of levels including through resident and business surveys, interviews, discussions with key community organizations, and a residents' workshop and open house. Key groups and agencies that participated in engagement activities included:

- Public;
- Communities in Bloom;
- Rotary Club;
- Wellness and Health Action Committee (WHAC);
- Winding Rivers Arts and Performance Society (WRAPS);
- Students and staff of Desert Sands Community School;
- Royal Canadian Mounted Police;
- Health Care Centre; and
- Parent Advisory Committee.

Initial Draft of Official Community Plan – an initial draft of the Official Community Plan was prepared based on the input of residents, businesses, and key stakeholders.

Planning Advisory Committee – a Planning Advisory Committee was formed to provide greater input on the development of the OCP. The committee was comprised of a diversity of representatives of key groups in the community including seniors, students, business owners, and young families. This committee met to review both the OCP and Zoning Bylaw and revisions to those documents were made based on the input provided.

Community Open House on Draft OCP – a community open house was held on May 29, 2018 to review the Official Community Plan. Two events were held on that day including an open house at the Ashcroft Hub and a presentation and open house at the Ashcroft Community Hall.

Revised draft of the OCP – a revised draft of the OCP was prepared based on the input of the participants in the OCP open house.

Agency Referrals – the OCP was provided to Interior Health, the Ministry of Transportation and Infrastructure, the Thompson-Nicole Regional District, School District 74, and the Agricultural Land Commission. Comments were reviewed and corresponding adjustments were made based on these.

First Nation Referrals – the OCP was provided to Ashcroft Indian Band for review.

Council Presentation – the OCP draft was presented on June 25th, 2018 at a Council meeting which community members attended.

1.5. Acknowledgments

The Village of Ashcroft would like to acknowledge the general support of the community throughout the OCP process. Their input and participation has been immensely beneficial. In particular, the Advisory Planning Committee, made up of a diverse group of community members, met a number of times throughout the process and provided detailed review. Their time and dedication to the process is greatly appreciated.

1.6. Organization of the Plan

This OCP is organized into four main parts with various sub-sections, including:



**Introduction
& Vision**

Part 1 – Introduction and Vision – this part of the OCP provides an introduction to the plan, a description of its purpose, a summary of the history of Ashcroft, a review of key community characteristics such as population demographics, and the community vision and guiding principles for the evolution of the community.



**Community
Development Plan**

Part 2 – Community Development Plan – this part of the OCP provides a description of how the community intends to evolve in the future. This includes a description of how the community will address the issues of economic development, infrastructure, environmental protection and emergency preparedness, energy and greenhouse gas emissions, community development, social needs, and housing.



Land Use Plan

Part 3 – Land Use Plan – the land use plan contains the land use designations used in the plan, the general policies associated with land use planning in Ashcroft, and the background, objectives, and policies in regard to the different designations. Part 3 also includes development permit area guidelines.



**Implementation
Strategy**

Part 4 – Implementation Strategy – this part of the OCP outlines an action and implementation plan for moving forward.

2. History of Ashcroft



The history of the Ashcroft area begins long before the Cariboo Gold Rush of the 1860s. Indigenous people have been living in the area since time immemorial. The local nations include the Nlaka'pamux Nation, which includes the present day First Nations of Ashcroft Indian Band, Oregon Jack Creek Indian Band, and Cook's Ferry; and the Secwepemc Nation which includes the Bonaparte Indian Band. People from the two nations intermarried frequently and their territories overlapped.¹

The Nlaka'pamux and Secwepemc people lived off the abundant fish (particularly salmon), wild animals, and edible and medicinal plants found in the area. In the winter, they made their home in the valley bottoms, living in pit houses. Remnants of these settlements can be found throughout the area today, and there is clear evidence of established intergenerational pit house settlements dating back over 5,000 years.² In the summer, Indigenous people in the area travelled further up into the hills for the purpose of hunting game.

Around the mid-1800s, the Nlaka'pamux people became involved in the European fur trade and increasingly incorporated agriculture into their livelihoods as their vast territories were reduced to reserves.

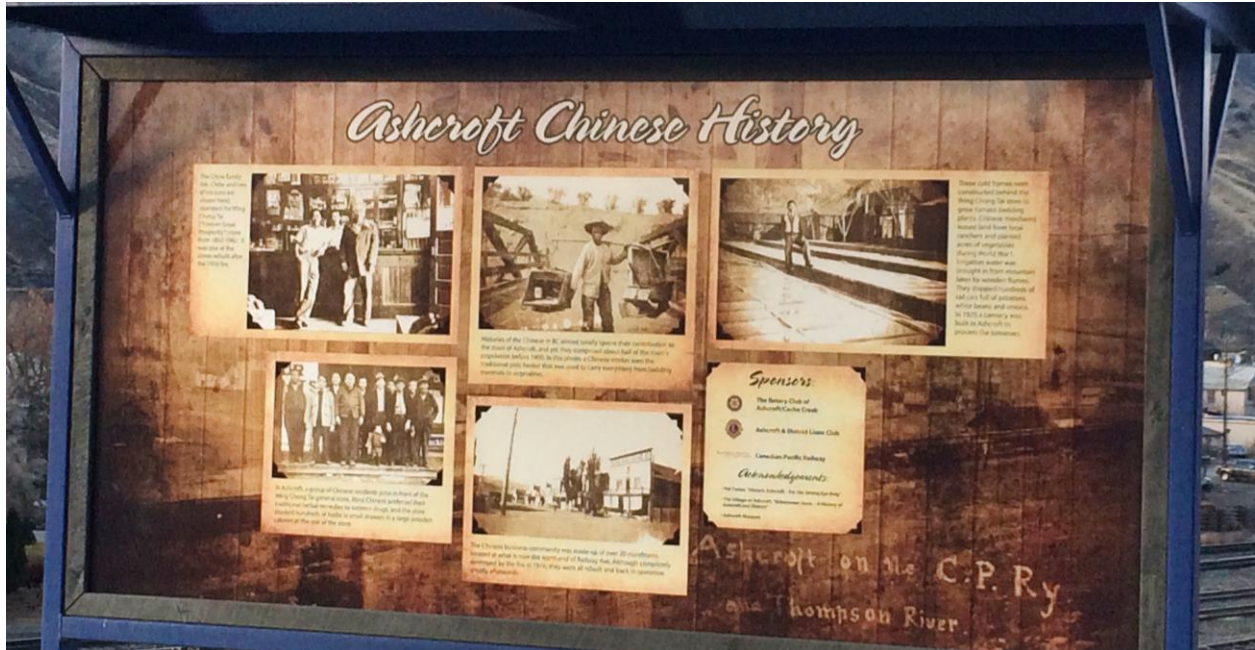
The first settlers of European descent in the Ashcroft area were Clement and Henry Cornwall, who arrived in 1859. Ashcroft is named for their home in England, Ashcroft Manor. JC Barnes and William Brink arrived

¹Ignace, Ron, 2008, *Our Oral Histories are our Iron Posts: Secwepemc Stories and Historical Consciousness*. PhD Dissertation, Simon Fraser University.

²Anderson Arnett, Christopher, 2016. *Rock Art of Nlaka'pamux: Indigenous Theory and Practice on the British Columbia Plateau*. PhD Dissertation, University of British Columbia.

in the area in mid-1860's and pre-empted land that would eventually become the townsite of Ashcroft. Here they began farming and grew grain, and fruit trees. They were soon joined by Oliver Evans and William Bose and the four became business partners in various undertakings.

Ashcroft has a unique colonial-era history that began with the community serving as a transportation hub in the 1800s. Since this time Ashcroft has experienced periods of boom and bust resulting in population growth and decline. The area served as Mile 0 for the road to the Cariboo goldfields and the BC Express Company was based here, providing transportation access into the Cariboo and Fraser-Fort George area. Ashcroft was also home to the iconic Ashcroft Manor, a roadhouse built by the Cornwall brothers.



In 1887 the town of Ashcroft was transformed by the completion of the Canadian Pacific Railway (CPR). It was then that the settlement was formally named Ashcroft and the rail company laid out a town plan to anticipate population growth as the area became the service centre for the Cariboo and northern areas of BC. By the 1910s however, the CPR was suffering due to the competition with the Pacific Great Eastern Railways and the Grand Trunk Pacific.

Ashcroft was incorporated as a Village in 1952. As time passed, Ashcroft came to rely on agriculture and a local cannery for economic stability. Once the cannery was closed in the late 1950s, the local economy became supported, in large part, by the Bethlehem copper mine, which began operating in 1962, the Lornex pit which began in 1970 and the Highmont Operating Corporation which operated from 1979 to 1984. The three operations eventually combined to become the Highland Valley Copper Mine.

The opening of the Coquihalla Highway in 1986 changed the economic dynamic of the Village once again by diverting substantial vehicle traffic away from Highway 1. More recently, there has been a growing emphasis on industrial activity related to the railway, such as the Ashcroft Terminal and Koppers Ashcroft Inc. Increasingly, the village has also become attractive to retirees looking to enjoy the local climate, friendly community and affordable lifestyle.

3. Technical Background

3.1. Demographics

Historical Population Growth

The current population of the Village of Ashcroft, according to a BC Stats estimate for 2017, is 1,531.³ The population has been declining in recent years. A number of factors have contributed to this including more workers at Highland Valley Copper Mine choosing to live in Kamloops rather than the small communities around the mine; the opening of the Coquihalla Highway which reduced traffic volumes on Highway 1, thereby reducing the potential for visitors to Ashcroft; and the reduction in government services in the area.

What makes Ashcroft unique:

In surveys and interviews, residents noted a number of characteristics which make Ashcroft a unique place to live. These include

- Climate
- Unique biophysical environment
- Community spirit
- History

Table 3.1 summarizes population changes by Census year since 2001.

Table 3.1: Historical Population Change by Census Year

	2001 ⁴	2006	2011	2016 ⁵
Population	1,814	1,664	1,628	1,558
% Change	-	-8.3%	-2.2%	-4.3%

Population Projections

Future population growth in Ashcroft will be influenced by the continued aging of the population, the availability of government services, particularly healthcare and education, and the success of industrial initiatives such as the Ashcroft Terminal and expansion of the Highland Valley Copper Mine. Factors which are negatively impacting population growth at this time are the changes in health and educational services as well as the availability of work in the community. At the same time, with an increasing number of residents at retirement age, the population has a lower capacity to replace itself in the future.

Population projections for the Village of Ashcroft have been made based on three scenarios:

Low: Using the annual average growth rate **(-0.66% per year)** for the village between 1986 and 2016.

Medium: Using the annual average growth rate used by BC Stats for projections for the South Cariboo Local Health Area⁶ between 2017 and 2037 **(-0.38% per year)**.

³ British Columbia, Population Estimates, 2017 Sub-Provincial Population Estimates. July 2017.

⁴ Population estimate according BC Stats based on 2006 boundaries. Statistics Canada, 2001 Census Profile http://www12.statcan.gc.ca/english/Profil01/CP01/Details/Page_cfm?Lang=E&Geo1=CSD&Code1=5933019&Geo2=PR&Code2=59&Data=Count&SearchText=ashcroft&SearchType=Begin&SearchPR=01&B1=All&Custom=

⁵ Statistics Canada, 2016 Census Profile. <http://www12.statcan.gc.ca/census-recensement/2016/dp-pd/prof/index.cfm?Lang=E>

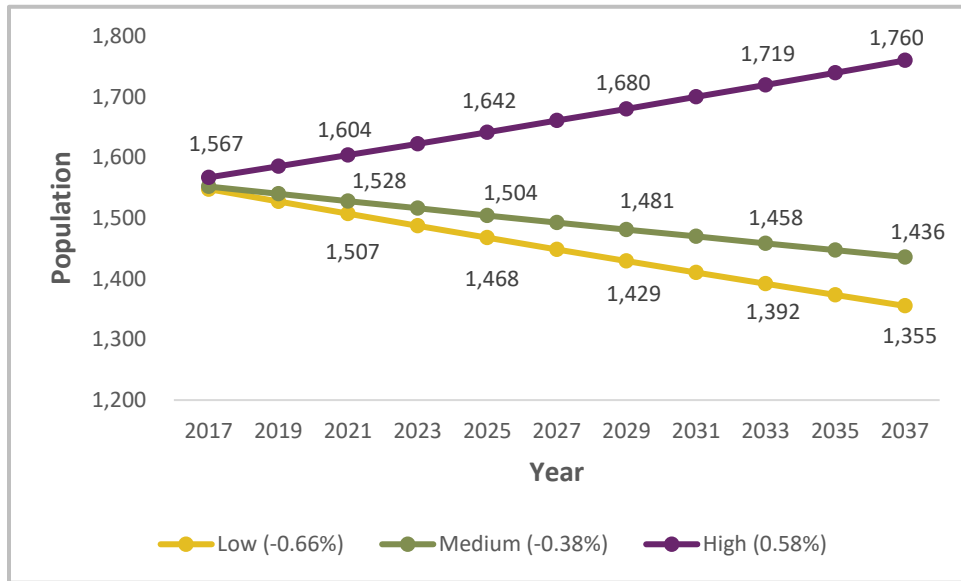
⁶ The South Cariboo Local Health Area stretches south and north of Ashcroft to encompass Lytton, Spences Bridge, Cache Creek, and Clinton. These small towns have been facing similar population trends to that of Ashcroft and are therefore a representative sample.

High: Using the annual average growth rate for the BC Stats projections for the Thompson-Nicola Regional District between 2017 and 2037 (**0.58% per year**).

In both the low and medium growth scenarios there is a future reduction in population. If population trends continue as they have for the past 30 years, the population of Ashcroft will be 1,355 people by 2037. However, considering the stability of key employers in the area such as Highland Valley Copper Mine and other manufacturing, a medium scenario with minimal loss may be more appropriate, leading to a reduction of fewer than 100 residents over the next twenty years. In the high growth scenario, the population could reach 1,760, which is still lower than the historical peak population of Ashcroft.

Figure 3.2 illustrates three different population projection scenarios for the Village of Ashcroft.

Figure 3.2: Population Projections



Future Development Considerations

In both the low and the medium population projection scenarios, the village will experience negative growth. However, in the high population projection scenario, an additional 202 people will reside in Ashcroft. Achieving a positive rate of population growth will depend on a number of factors, including:

- A higher number of young Ashcroft residents staying in or returning to the village in adulthood;
- An increased number of manufacturing or mine employees taking up residence in the village;
- Agricultural employees (i.e. Desert Hills Farm) becoming permanent residents; and/or
- The establishment of new employers in Ashcroft.

Age Profile

A comparison of the age profile of Ashcroft with that of the TNRD and BC shows that a higher proportion of village residents are 65 years and over than in the regional district, and that a relatively lower proportion of residents are under 45 years of age. The proportion of residents between 45 and 64 years is almost equal in all three jurisdictions. As indicated in **Table 3.2**, the median age in Ashcroft is over 15 years higher than in the province as a whole. **Table 3.2** compares the age profile of Ashcroft with that of the TNRD and the province of BC.

Table 3.2: Comparative Age Profiles 2016

Age Group	Ashcroft	TNRD	BC
0-14 years old	12%	15%	15%
15 – 24 years old	6%	11%	21%
25 – 44 years old	16%	23%	26%
45 – 64 years old	30%	30%	29%
65+	35%	20%	18%
Total Population	1,558	132,663	4,648,055
Median Age	58.6	45.2	43

3.2. Economy

Employment

According to the 2016 Census, the total number of Ashcroft residents in the workforce was 635. Of that number, 585 were employed. Employment is centred around manufacturing (i.e. IG Machine and Fibres, Koppers), transportation (i.e. Ashcroft Terminal), agriculture (Desert Hills Ranch employs up to 150 people on a seasonal basis), and services such as health care and schools. There is also some hospitality-related employment, such as the Ashcroft River Inn, Sundance Guest Ranch (located outside the village boundaries) and two bed and breakfasts in the village itself. Some of the major employers in Ashcroft include:

Industry/Transportation

- Arrow Transportation;
- Ashcroft Terminal;
- Highland Valley Copper – Teck;
- IG Machine and Fibres; and
- Ashcroft Treating Plant (Koppers Inc.).

Agriculture

- Desert Hills Ranch

Commercial

- Safety Mart Foods.

Services

- School District 74;
- Ashcroft Hospital and Community Health Care Centre; and
- Village of Ashcroft.



Income

Income levels are an important indicator of Ashcroft’s economic health. There are signs of an increase in income in Ashcroft in recent years. Median household total income in 2010 was \$47,109, and \$61,248 in 2015.⁷ In the TNRD, total household income rose from a median of \$59,385 to \$69,308. While median income in Ashcroft is still almost \$10,000 less than in the TNRD, it rose by over \$12,000 between 2010 and 2015, an increase of approximately \$2,000 more than in the TNRD.

3.3. Existing Development

Residential

Ashcroft’s residential areas include the Mesa Vista subdivision, Downtown and North Ashcroft. There is also a mobile home park to the south on the east side of the Thompson River.

⁷ Income data is from the year before the censuses took place, 2010 and 2015.



There are 755 occupied dwelling units in Ashcroft, according to the 2016 Census. In the Village of Ashcroft, the predominant housing or dwelling type, according to the 2016 Census⁸ is the single-detached house, which accounts for 72% of all dwellings. Other dwelling types found in Ashcroft to a lesser extent are semi-detached houses, row houses, and movable dwellings (mobile homes).

From a housing perspective, there is an average of 2 people per household. If an optimistic growth scenario is achieved, there will be an additional 202 people in Ashcroft that will need housing. This would result in a need for 101 new dwelling units in Ashcroft in the next 20 years. To meet this demand there would need to be a mix of single family and multi-family units as well as housing specific for seniors.

Currently there is potential for a sufficient number of lots to be created on developable land formerly designated for public and institutional uses in the

Mesa Vista area to enable development of more than 25 single family lots, which would address any growth in the next five years. There is also potential for densification in the Downtown and North Ashcroft through the addition of secondary suites and infill development.

⁸ Statistics Canada, 2016 Census Profile. <http://www12.statcan.gc.ca/census-recensement/2016/dp-pd/prof/index.cfm?Lang=E> (accessed August 24, 2017).



Commercial

Commercial development is concentrated in the Downtown, particularly along Railway Avenue. Railway Avenue continues to be seen as the 'Main Street' area with a continuous series of storefronts. The downtown also has a number of historical buildings that draw visitors and helps contribute to Ashcroft's distinctiveness as a community.

Industrial

Industry is found in three areas: south of downtown on the east side of the Thompson River, along the riverfront on Hollis Road, and northeast of downtown on the south side of the Thompson River. The industrial area located northeast of the community is used for the Ashcroft Terminal, which takes advantage of having both the CN and CP rail lines on this site.

Agricultural

Agriculture has traditionally been an important component of Ashcroft's economy. A significant proportion of land within the Village boundaries is within the Agricultural Land Reserve. A part of this is accounted for by Desert Hills Ranch land in North Ashcroft. This operation grows produce including watermelon, cantaloupe, tomatoes, onions, peppers and beets. There is also land used for grazing livestock within the Village limits. The Blue Goose Cattle Company leases the Ashcroft Ranch and raises organic beef for commercial purposes.

Institutional

Traditionally, Ashcroft was a provincial government sub-regional centre, and this continues today with School District No. 74 offices and the Ashcroft Hospital and Community Care Centre both located in the community. In addition, the Ashcroft detachment of the RCMP and other government services including Community Futures and Aboriginal Skills and Employment Training Strategy centre are also located in Ashcroft.

Parks and recreational uses also occupy a significant portion of the land base in Ashcroft. This includes indoor facilities such as the Ashcroft HUB and the Drylands Arena, as well as outdoor facilities including Legacy Park, soccer fields, tennis courts, a skateboard park, and an outdoor pool.

3.4. Environmental Characteristics

The Village of Ashcroft is located in the Thompson River valley which is characterized by unstable soils, steep slopes and riparian areas.

Steep Slopes – there are steep slopes on the south side of the Thompson River in downtown Ashcroft adjacent to the river, as well as between the downtown and Mesa Vista. In North Ashcroft, steep slopes line the Thompson River as well as along the northeast boundary of the village to Highway 97.



Unstable Soils - unstable soils are located north of a band of steep slopes in the North Ashcroft area, reaching the northern boundary of the village. They are also located to the northeast, east, and south of the Mesa Vista subdivision, and another small subdivided area of rural residential located to the northeast of the Mesa Vista subdivision.

Floodplain - a narrow floodplain borders the Village of Ashcroft along the Thompson and Bonaparte rivers. The floodplain is located next to a steep slope which borders the downtown to the south and residential and industrial areas in North Ashcroft.

Riparian Areas – the Bonaparte and Thompson rivers meander through Ashcroft and have important riparian areas including at the confluence of the two rivers in North Ashcroft.

Schedule C illustrates these areas.

3.5. Existing Community Services



Key community services provided in Ashcroft by the Village and other service providers include:

- Recreation;
- Fire Protection;
- RCMP Detachment;
- Solid Waste Management (curbside solid waste collection with a recycling depot);
- Animal Control;
- Street Lighting;
- Transit; and
- Library.

3.6. Existing Infrastructure

Infrastructure in Ashcroft includes the following:

Water System

The Ashcroft water system services all of the main developed areas within the municipal boundaries. In 2018, the Village improved the quality of their drinking water by installing a membrane filtration system. These improvements increased the system's capacity to cope with water turbidity and other incidents that lead to a higher level of water particulates and bacteria. The drinking water now meets current provincial drinking water guidelines. The site of the filtration system is directly behind the main pump station adjacent to Legacy Park Campground.

Sewer System

The community sewer system serves all residents that live in the higher, medium density and low density residential areas. The sanitary system also serves the mobile home park. The sewage is pumped via a series of lift stations to the sewage treatment plant located along Highway 97C.

Transportation



Key transportation in Ashcroft includes roadways, railways, air, and bus:

- **Roadways** - The Village of Ashcroft is located east of Highway 1 and is bisected from the north by Highway 97C. There is also access to Highway 1 via Cornwall Road through Ashcroft IR # 4. Important routes in Ashcroft include Railway Avenue, which is a commercial and services hub area, and Mesa Vista Drive on the east side of the Thompson River.
- **Railways** - The railways are a key characteristic of Ashcroft. The CN Rail line borders the east and north side of the Thompson River before crossing over to the Ashcroft Terminal area approximately 2 km north of the main village area. The CN station is located off Hollis Road. Via Rail also uses this line and has a stop in Ashcroft. This service runs the eastward bound service Tuesday, Friday, and Sunday May through October, and Tuesday and Friday in the off-season. In the westward direction it runs Tuesday, Thursday, and Saturday in the high-season and Tuesday and Saturday in the off-season. The CP line borders the opposite side of the Thompson River heading northeast where it meets the CN line at the Ashcroft Terminal Site. The Ashcroft Terminal performs transloading service and railcar storage.
- **Airport** - The closest major airport to Ashcroft is the Kamloops Airport, located nearly 100 km from Ashcroft. There is also the Cache Creek Airport located roughly 6 km from Ashcroft, but it is not frequently used.
- **Bus** – Another means of transportation in Ashcroft is by bus. BC Transit runs the Ashcroft-Clinton Transit System. Route 1 runs between Ashcroft and Clinton on Wednesdays and Fridays, leaving twice daily. There is also a weekly trip to Kamloops on Mondays and a monthly trip to 100 Mile House on the first Monday of every month. Greyhound bus service is available only from Cache Creek.

Recreation

A large recreational complex is located at the corner of Elm Street and Government Street. From the May long weekend to the Labour Day weekend, an outdoor swimming pool operates here. There are also sports fields and a baseball diamond located in North Ashcroft. The Ashcroft Curling Club is located on 7th Street and is adjacent to the Drylands Arena, which operates October through March. Heritage Place Park is a picnic area for tourists and locals that provides seasonal tourist information and public washroom facilities. The Canada Day celebrations and Music in the Park events happen here throughout the summer months. There are recreational trails in the hills surrounding the community as well as a skateboard park located in the downtown area.

3.7. Indigenous Communities

First Nations continue to live in the area and are an important part of the Ashcroft community. The First Nations with the closest proximity to the Village of Ashcroft are the Nlaka'pamux Nation communities of Ashcroft Indian Band and Oregon Jack Creek Indian Band and the Secwepemc Nation community of Bonaparte Indian Band.

Ashcroft Indian Band

Ashcroft Indian Band's total membership population is 279.⁹ The number of those which live on an Ashcroft Indian Band reserve is 72. Tragically, 13 homes were destroyed during the 2017 wildfires, fortunately there was no loss of life. Ashcroft Indian Band is part of the Nlaka'pamux Nation but is not affiliated with any tribal council or association.

Oregon Jack Creek Indian Band

Oregon Jack Creek Indian Band is located approximately 6 km south of Ashcroft. The total membership population is 68 people¹⁰ and most reside off-reserve. The remaining 16 people live on Paska Island IR #3 and Upper Nepa IR # 6.¹¹ Oregon Jack is part of the Fraser Thompson Indian Services Society and the Nlaka'pamux Nation Tribal Council.

Bonaparte Indian Band

Bonaparte Indian Band is located approximately 15 km north of Ashcroft. Bonaparte Indian Band is a member of the Secwepemc Nation and the Shuswap Nation Tribal Council. A total of 947 people are members of the Bonaparte Indian Band.¹² Of these, 149 live on a Bonaparte reserve, 50 live on other reserves, and 748 live off-reserve.

⁹ Indigenous and Northern Affairs Canada, First Nation Profiles. http://fnppn.aadnc-aadnc.gc.ca/fnp/Main/Search/FNRegPopulation.aspx?BAND_NUMBER=685&lang=eng

¹⁰ Indigenous and Northern Affairs Canada, First Nation Profiles. http://fnppn.aadnc-aadnc.gc.ca/fnp/Main/Search/FNRegPopulation.aspx?BAND_NUMBER=692&lang=eng

¹¹ BCStats, 2016 Census Total Population Results, Indian Reserves.

<https://www.bcstats.gov.bc.ca/StatisticsBySubject/Census/2016Census/PopulationHousing/IndianReserves.aspx>

¹² Indigenous and Northern Affairs Canada, First Nation Profiles. http://fnppn.aadnc-aadnc.gc.ca/fnp/Main/Search/FNRegPopulation.aspx?BAND_NUMBER=686&lang=eng

4. Vision and Guiding Principles

4.1. Community Vision - 2038

Based on community input, the vision for Ashcroft is:

In 2038, Ashcroft is a vibrant, active and creative community that fosters healthy well-being and a collaborative inter-generational approach to our community's development.

Our vision includes the following aspects:

- **Community Oriented** – We have a strong volunteer base and we attract people to live here because there are exciting opportunities for people to have an impact on their community.
- **Diverse Population** – Our community is inter-generational and has a good mix of all demographics.
- **Creative Community** – There is a strong creative community that enables us to develop novel approaches to the issues we face.
- **Technology and Industries** – We will welcome a diversity of new technologies and industries to our community.
- **History** – The Indigenous and non-Indigenous history of our community is recognized and celebrated.
- **Wellness** – The physical and mental well-being of residents is well supported in the community through the built and natural environments as well as the provision of a wide range of health and wellness services suitable for people of all ages.
- **Small Town Atmosphere** – We are a place where people know each other and where there is a strong sense of interconnection.
- **Economically Resilient** – We have a diverse range of creative entrepreneurs who have integrated themselves into the community and thrive in our small-town atmosphere.
- **Neighbours** – We work collaboratively with our neighbours to strengthen our community and our region.



4.2. Guiding Principles

Guiding principles provide direction to the adoption and implementation of the OCP and include:

- Community Building
 - The Village will support initiatives that result in the building and strengthening of the community. This includes encouraging and supporting the efforts of volunteers making meaningful contributions to improving the quality of life in Ashcroft.
- Common Sense
 - Solutions to key issues will be practical and within the capacity of the community to achieve them. This will include recognizing the opportunities and constraints with respect to the resources that we have available to invest in various initiatives. Efforts will be made to increase the awareness of residents on municipal financial literacy and development processes. It also means that we will have a common-sense approach to development regulations.
- Innovation
 - The Village will support and encourage innovative solutions to the issues that Ashcroft faces. This can include new technologies, processes and concepts that lead to the strengthening of our community.
- Partnerships
 - The Village will leverage partnerships with non-profit agencies, local, provincial and federal government, businesses and Indigenous communities to achieve key community aspirations.
- Reconciliation
 - The Village will pursue initiatives that support ongoing efforts of reconciliation with Indigenous communities.
- Economic Development
 - The Village will pursue initiatives that aim to improve the economy of Ashcroft in a sustainable manner that will result in new jobs, both permanently and temporarily, and expands the tax base for the community.
- Role of the Municipality
 - The Village can play many different roles and is not necessarily responsible for leading the implementation of all aspects of the Official Community Plan but can be a sponsor, supporter, facilitator, and/or funder depending on the initiative.
- Maintain the Character of the Community
 - Ashcroft combines history and the natural environment to create a unique community. Ashcroft respects the environment by protecting and enhancing natural assets while limiting the impact of the built environment on the natural environment where possible. In addition, the Village will encourage new development to fit within the existing character of development in Ashcroft.

5. Regional Context Statement

As required by the Local Government Act, municipalities must include a regional context statement in their Official Community Plans if their regional district has adopted a regional growth strategy. The purpose of the regional context statement is to identify how the OCP policies support the principles of the regional growth strategy.

In 2000, the Thompson Nicola Regional District (TNRD) adopted a Regional Growth Strategy that outlines a cooperative strategy for achieving a sustainable future for the region. In 2013 the Growth Strategy was updated. The policies contained in the Regional Growth Strategy support the TNRD's vision statement, which reads:

The TNRD is a unique region of diverse urban and rural communities, wilderness and natural resource opportunities, and a vibrant economy. Building on this diverse mix of urban and rural, resource and wilderness settings shall be the dominant guiding principle as new development is encouraged. Urban places will grow while retaining their existing scope and character; rural places will remain rural while accepting appropriate new development; and settlement in resource areas will be limited and carefully managed. (Bylaw No. 2409, 2013, p. 3)

Ashcroft's Regional Context Statement

The Village of Ashcroft's regional context statement was developed in accordance with the TNRD's Regional Growth Strategy. The following sub-sections describe how the Village of Ashcroft's OCP supports the TNRD's growth management strategy.

Human Settlement

The Human Settlement section of the TNRD's Regional Growth Strategy contains policies that aim to contain urban/rural sprawl, considering their projections add a total of 29,000 people who would mostly settle in Kamloops and Merritt. The aim of the TNRD is to build on the existing network of diverse regional centres, directing growth to established centres, promoting infill and intensification, and ensuring adequate levels of servicing are provided. The Village of Ashcroft's OCP supports the TNRD's Human Settlement policies by:

- Encouraging compact and cost-effective residential development by directing growth to established residential areas;
- Ensuring there is an adequate supply of land to meet future housing demand;
- Permitting secondary suites and carriage suites in Rural and Low Density Residential;
- Promoting mixed-use and medium density development downtown;
- Promoting medium density development in North Ashcroft;
- Consciously blending commercial with residential and compatible industrial uses to maintain the downtown as a livable and vibrant place;
- Protecting environmentally sensitive areas, particularly along the Thompson and Bonaparte Rivers with Development Permit Guidelines; and
- Encouraging a range of housing types including housing cooperatives, seniors housing, rental housing, and accommodation for temporary workers.

Energy and Transportation

The Village of Ashcroft also supports the TNRD's Regional Growth Strategy objective to integrate transportation and energy considerations with land use and settlement planning to achieve conservation, mobility, and efficiency goals. The OCP includes policies that:

- Encourage infill in residential development to allow for compact community design and efficient use of transportation infrastructure;
- Encourage the development of trails for walking or cycling between key locations;
- Develop an electric vehicle charging station in the community;
- Work towards improving transit ridership levels by collaborating with BC Transit to improve the user experience; and
- Encourage solar energy use in the community.

Economic Development

The Village's OCP contains policies that support the Regional Growth Strategy's policy to broaden the economic base through diversification and expansion, and support and encourage existing primary, secondary, and tertiary industries while promoting new economic development opportunities. The OCP supports the Regional Growth Strategy by:

- Encouraging historically themed architecture into new and existing development Downtown to promote tourism;
- Encouraging the further development of the Ashcroft Terminal lands as a site of future industrial development;
- Encouraging health professionals to move to Ashcroft to promote high service levels and maintain the population;
- Encouraging residents to shop locally wherever possible;
- Promoting and marketing Ashcroft as a tourism destination; and
- Working with local agricultural operations to identify opportunities to help market and promote them as part of overall economic development initiatives.

Environmental Protection

The Village of Ashcroft supports the TNRD's goal to protect and enhance the environment through the adoption and co-operative use of stewardship principles. The OCP includes policies that:

- Designate environmentally sensitive areas and apply Development Permit Guidelines to help ensure that development does not negatively impact environmentally sensitive areas;
- Reduce Ashcroft's impact on the environment and protect the surrounding ecosystems and wildlife; and
- Ensure industrial development complies with applicable environmental standards.

Open Space and Cultural Heritage

The Village of Ashcroft's OCP supports the Regional Growth Strategy's policies to protect the archeological and heritage resources, open space, and rural character of the region by:

- Encouraging owners of heritage homes to maintain the heritage aesthetic;
- Continuing the recognition of significant heritage buildings through the historical plaque program; and
- Designating lands for Open Space to provide recreation opportunities and to protect environmentally sensitive areas.

Co-operation and Process

The Village of Ashcroft's OCP has been developed in consultation with the TNRD to ensure that the OCP is consistent with the Regional Growth Strategy. The Village has also adopted an OCP Consultation Policy that will ensure that the appropriate governments, government agencies, First Nations, and other stakeholders are properly consulted in the process of developing, amending, or repealing an OCP.





Community Development Plan

Part 2 of the OCP includes the development of goals and policies pertaining to key Community Development topics such as economic development, environmental protection and emergency preparedness, community safety, community development, social needs, and housing. Finding solutions to issues in each of these topic areas will be critical for the long-term vitality of Ashcroft.

6. Economic Development

The Village of Ashcroft has a number of important assets in terms of its location, community, and existing economic activity, which will play a role in supporting economic development in the future. While the Village's boundaries encompass portions of Highway 1, the townsite itself is located within minutes of the highway, which connects the Lower Mainland to the Interior, making the community suitable for the transportation of goods. In addition to manufacturing, tourism also benefits from the Village's proximity to Highway 1, as it brings in people interested in the Village's history, arts community, or the peace and quiet of Ashcroft.

Agriculture in Ashcroft is an important economic sector. Ashcroft has the benefit of one of the warmest climates in BC, as well as ample water for production. Manufacturing in Ashcroft is also succeeding due to access to important transportation infrastructure. Ashcroft also has a transloading terminal within its boundaries which enables the loading of unloading of train cargo to trucks. This facility is one of the only locations in western Canada that has both CP and CN rail lines.

A key challenge faced in Ashcroft is the lack of qualified personnel living locally. One difficulty associated with establishing a local workforce is the lack of appropriate housing. With rental units and market housing in short supply, this could be a barrier to future expansion in labour-heavy industries such as agriculture, manufacturing or mining. Another challenge is that many workers choose to reside in Kamloops and commute to Ashcroft for work. This allows them to take advantage of the broader range of services and amenities available in a larger community.

Another aspect of economic development is the attraction and retention of people in the community, and encouraging the community to support local businesses. The number of businesses along Railway Avenue has diminished over recent years, due partly to fire, but also due to the difficulty in attracting businesses to a small community. A challenge moving forward will be finding people to own and operate businesses in Ashcroft.

6.1. Goals

It is the goal of Council to:

- .1 Encourage a more vibrant and diversified economy that attracts entrepreneurs that will contribute to the Ashcroft community;
- .2 Develop and maintain a positive business environment;
- .3 Encourage the further development of the tourism industry; and
- .4 Encourage the further development of the agricultural industry in Ashcroft.

6.2. Policies and Actions

It is the policy of Council to:

- .1 Diversify the economy by:
 - .1 Reviewing options to market access to rail, highway, and fibre optic technology to attract new businesses to Ashcroft;

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- .2 Encouraging more health professionals to permanently relocate to Ashcroft to create jobs in the community and to provide services that residents require;
 - .3 Working with major employers to explore options to market Ashcroft and its quality of life to skilled workers;
 - .4 Reinforcing industrial areas to be used primarily for industrial purposes with residential as a secondary and accessory purpose;
 - .5 Continuing to implement the recommendations from the Village of Ashcroft Economic Development Strategy; and
 - .6 Developing an online economic development focused Community Profile that markets Ashcroft's economic advantages and contains all related bylaws (i.e. OCP and Zoning).
- .2 Develop and maintain a positive business environment by:
 - .1 Supporting the utilization of the Ashcroft HUB as a business incubator while encouraging businesses to eventually locate in downtown spaces;
 - .2 Encouraging the reinvigoration of the Chamber of Commerce to represent the business community;
 - .3 Working collaboratively with prospective businesses to find creative solutions that reduce barriers to establishing, operating, or expanding a business in Ashcroft;
 - .4 Encouraging residents to shop locally wherever possible; and
 - .5 Considering allowing a greater range of businesses in the Downtown Area.
 - .3 Encourage the further development of the tourism industry by:
 - .1 Working with key partners such as nearby Indigenous communities, the Thompson Nicola Regional District, Gold Country Communities Society, nearby municipalities and other key stakeholders to develop an integrated tourism strategy that leverages Ashcroft's agriculture, history (Indigenous and non-Indigenous), scenery, and the arts and culture community and provides direction on promotions and marketing;
 - .2 Continuing to operate the municipal campground and consider upgrades to the facility;
 - .3 Enabling permanent residents of residential dwelling units to operate Short-term Rental Accommodations in order to support the development of the tourism industry while monitoring and adjusting for potential impacts to long-term housing availability;
 - .4 Installing visually attractive signs on Highway 1 to encourage motorists to come into Ashcroft;
 - .5 Promoting and marketing Ashcroft as a tourism destination;
 - .6 Reviewing the feasibility of developing a sister city relationship with a Mexican community recognizing the significant role that temporary workers from Mexico play in the community;
 - .7 Continuing to support the role of the arts in bringing people to Ashcroft;
 - .8 Considering further investments in downtown beautification and streetscaping;
 - .9 Continuing to utilize the Northern Development Initiative Trust to access programs to enhance commercial development in Ashcroft, such as the façade improvement program; and
 - .10 Continuing to encourage the relationship with the Village's Japanese Sister-City, Bifuka, Japan.

- .4 Encourage the further development of the agricultural industry by:
 - .1 Reviewing opportunities to leverage Ashcroft's agriculture sector to create jobs and generate spin-off activities such as value-added processing and agri-tourism, as permitted by the Agricultural Land Commission;
 - .2 Encouraging the development of a Farmers Market;
 - .3 Encouraging the use of underutilized agricultural land, particularly for crops with emerging demand such as grape production and wineries, hops farms, and produce; and
 - .4 Working with local agricultural operations to identify opportunities to help market and promote them as part of overall economic development initiatives.



7. Environmental Protection and Emergency Preparedness

Ashcroft is committed to preserving its environment for the enjoyment of this generation and future generations. Key issues that Ashcroft faces today are related to protecting water quality, ensuring that air quality supports the health of the entire community, respecting the wildlife which make their home in and around Ashcroft, and generally reducing the community's environmental footprint.

The wildfires in the summer of 2017 gave rise to a number of questions within the community about the emergency preparedness of the community and its capacity to respond quickly either to a fire or another event which prompts evacuation alerts and orders. There are potentially opportunities to improve the Village's and key agencies' communication capabilities and protocols, and more clearly define key roles and responsibilities to ensure that the community is aware of an alert or evacuation. Due to climate change, the likelihood of future natural disasters such as wildfire and flooding has increased and therefore it is important that the community is better prepared for emergencies that may arise.

7.1. Goals

It is the goal of Council to:

- .1 Work towards protecting and enhancing the natural environment;
- .2 Prepare for emergencies that Ashcroft may encounter due to natural disasters; and
- .3 Reduce the impacts of the built environment on the natural environment.

7.2. Policies and Actions

It is the policy of Council to:

- .1 Work towards protecting and enhancing the natural environment by:
 - .1 Working to ensure that the impacts of development on environmentally sensitive areas are minimized to the greatest extent possible;
 - .2 Reducing negative impacts on the climate;
 - .3 Promoting xeriscaping, or drought-resistant landscaping;
 - .4 Ensuring that riparian areas are protected appropriately through the use of Development Permit Area Guidelines;
 - .5 Promoting the conservation of fish habitat on the Thompson and Bonaparte Rivers;
 - .6 Requiring developments to adhere to applicable senior government legislation to protect watercourses and fish habitat from urban encroachment;
 - .7 Protecting public safety and minimizing property damage associated with flooding events by encouraging flood susceptible areas to be used as park, recreational, or conservation areas;
 - .8 Supporting the development of an Urban Tree Program to provide habitat for birds and other wildlife, improve air quality, reduce greenhouse gas emissions, and improve the tree canopy in Ashcroft; and
 - .9 Considering options to remove non-native vegetation from parks and community spaces.

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- .2 Reduce the impacts of the built environment on the natural environment by:
 - .1 Supporting efforts to reduce solid waste by continuing to offer and potentially expand recycling programs and considering the development of a composting program;
 - .2 Reducing greenhouse gas emissions through energy conservation; and
 - .3 Practicing water conservation to reduce water consumption in the community and retain water for important natural needs.
- .3 Prepare for emergencies that Ashcroft might encounter due to natural disasters by:
 - .1 Updating the emergency plan for Ashcroft based on the lessons learned during the 2017 wildfires;
 - .2 Considering how to integrate Fire Smart guidelines into Ashcroft governance;
 - .3 Supporting public Fire Smart education in the community leveraging resources from the provincial government;
 - .4 Supporting the installation of a centrally located signboard which could communicate community events as well as evacuation alerts and orders;
 - .5 Developing an emergency access route out of the Mesa Vista neighbourhood;
 - .6 Developing and communicating best practices for the emergency preparedness of each household in Ashcroft;
 - .7 Reviewing opportunities to have an emergency generator installed in Ashcroft for key facilities and infrastructure; and
 - .8 Increasing public education on water conservation and water consumption for residents during natural disasters.

8. Energy and Greenhouse Gas Emissions

Reducing greenhouse gas (GHG) emissions is a key goal for the Village of Ashcroft. In 2012, according to the Community Energy and Emissions Inventory (CEEI), GHG emissions associated with buildings and solid waste in the community were 4,511 tonnes/CO₂E, which represents a 20% reduction from 2007 (transportation-related emissions were not calculated in the CEEI).

The Village recognizes that it has an important role to play in leading the community to meeting these targets. That being said, the attainment of these targets is also conditional upon the Province pursuing various initiatives such as changing the BC Building Code to require the incorporation of green building technologies for new buildings and renovations, and enabling the creation of new, alternative, fuel efficient, and renewable energy sources to power vehicles and heat homes. Without provincial initiatives and advances in technology, reducing GHG emissions will be challenging.

A related initiative for Ashcroft will be to continually make strides in ensuring that energy is provided in a more sustainable manner. This includes both seeking greater energy conservation while also reviewing opportunities to increase the use of alternative sources of energy. The Village is a solar community and has adopted a solar ready regulation bylaw to require that new development is constructed to enable the addition of solar hot water technology. The Village has already undertaken some initiatives in this regard including the installation of a solar hot water heating system on the pool and would like to install an electric vehicle charging station. The plans for the design of the water treatment plant include the installation of solar panels.

8.1. Goals

It is the goal of Council to:

- .1 Strive to reduce greenhouse gas emissions in the community by 10% below 2012 levels by 2025; and
- .2 Encourage energy conservation and alternative energy sources in the community.

8.2. Policies and Actions

It is the policy of Council to:

- .1 Reduce greenhouse gas emissions in the community by:
 - .1 Providing educational materials to residents that outline how they can reduce their greenhouse gas emissions;
 - .2 Considering the adoption and enforcement of an anti-Idling program;
 - .3 Reviewing the feasibility of allowing electric golf carts on public roads as another mode of travel;
 - .4 Considering the installation of an electric vehicle charging station in the community;
 - .5 Developing trails and other features that support people walking or cycling between key locations;
 - .6 Working towards improving transit ridership levels by collaborating with BC Transit to improve the user experience;

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- .7 Requiring trails, pathways and/or sidewalks to be incorporated within the design of new subdivisions;
 - .8 Reviewing the feasibility of providing incentives to builders who choose to use alternative energy systems such as solar hot water and geo-exchange heating;
 - .9 Encouraging residents and businesses to conduct energy audits and to undertake energy retrofits;
 - .10 Providing incentives to builders and property owners to construct more energy efficient homes and retrofit existing homes to make them more energy efficient;
 - .11 Requiring new homes that are the subject of rezoning applications to meet green building standards;
 - .12 Continuing to direct development away from the Agricultural Land Reserve in order to protect and support local agricultural production;
 - .13 Working with the Thompson Nicola Regional District to examine options to reduce quantities of waste and to provide alternatives for waste disposal;
 - .14 Continuing to support recycling initiatives;
 - .15 Reviewing the feasibility of developing a composting program;
 - .16 Considering the development of community garden sites on Village lands and raising awareness about the value of purchasing locally-produced food and supporting programs that build a robust local food network; and
 - .17 Requesting the Province to provide the Village with the tools and resources to monitor and measure its GHG levels.
- .2 Encourage energy conservation and the use of alternative energy sources in the community by:
- .1 Reviewing the feasibility of installing alternative energy systems into municipal buildings, such as solar hot water, solar electricity, and geo-exchange systems;
 - .2 Encouraging the installation of solar panels on houses, businesses, and government buildings;
 - .3 Encouraging energy efficiency in the community and review Village-owned facilities for efficiency options; and
 - .4 Reviewing the feasibility of developing community energy systems by considering the use of solar and/or wind energy.

9. Community Development

Community development refers to the process of taking community actions which prioritize social, economic, environmental, and cultural well-being; and their contributions to improving the community as a place to live. The promotion of community development offers many benefits. Encouraging diversity and inclusion in terms of ethnicity, gender, and age leads to feelings of inclusion and the social cohesiveness of a community. Events and activities which celebrate this cohesiveness in the community already are sporting events, community gardens, and artistic activities. An example of a project trying to create social unity in Ashcroft is the creation of mosaics which celebrate all aspects of the community's heritage. Activities which are organized by the Winding Rivers Arts and Performance Society (WRAPS) also bring different generations together, and even different communities together, as all are welcomed to be a part of their productions.



The Ashcroft HUB has become integral to Ashcroft by bringing different generations together on a daily basis. Offering a variety of recreational and social activities catering to all demographics and interests may prove to be an important part of retaining families in the community. The benefits of increased social cohesion are pride of place, a desire for the Village to be attractive to visitors, a desire to support local businesses and provide local employment, and support for local food production.

In terms of community assets, moving forward it will be vital to attract and retain young families, as well as encourage young Ashcroft residents to move home after completing their education elsewhere. The availability of amenities and recreational opportunities which responds to the needs of all generations living in Ashcroft will be key to the community's future viability and vibrancy. Services such as education and health care, while provided at a provincial level, should be encouraged and promoted in order to maintain current levels and make improvements where possible.

9.1. Goals

It is the goal of Council to:

- .1 Strengthen community cohesiveness;
- .2 Support the development of the arts and culture community in Ashcroft; and
- .3 Enhance food security in Ashcroft.

9.2. Policies and Actions

It is the policy of Council to:

- .1 Strengthen community cohesiveness by:
 - .1 Encouraging activities and programs which promote diversity and inclusion;
 - .2 Recognizing the Ashcroft HUB as a vehicle for the promotion of social cohesiveness;
 - .3 Recognizing and celebrating the role of Indigenous people in this area, both historically and currently;
 - .4 Working with key stakeholders and partners to develop more community events that embrace inclusion by bringing people together;
 - .5 Reviewing options that could lead to improved communications in the community between the municipality and the people;
 - .6 Strengthening working relationships with and between volunteer groups in Ashcroft;
 - .7 Reviewing options to hire a grant writer with support from the Northern Development Initiative Trust to help local community groups access funding for their programs;
 - .8 Working with key stakeholders to determine options to better integrate temporary workers into the community and to promote Ashcroft as a place to live permanently;
 - .9 Implementing rainbow crosswalks in the village to signify support for the LGBTQ community; and
 - .10 Working with neighbouring communities and local governments (such as the TNRD) to share resources in the delivery of recreational programming and activities.
- .2 Support the development of the arts and culture community in Ashcroft by:
 - .1 Encouraging WRAPS and other community organizations to continue their programs which promote social interconnection and inclusivity;
 - .2 Utilizing the arts to promote cross-cultural understanding;
 - .3 Incorporating Indigenous and ethnic art and culturally based designs into civic, institutional, public and park spaces; and
 - .4 Encouraging the ongoing creation and installation of public artwork.

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- .3 Enhance food security in Ashcroft by:
 - .1 Promoting the development of a community garden;
 - .2 Encouraging the sharing of surplus fruits and vegetables with the development and promotion of a Gleaning Abundance Program;
 - .3 Investigating becoming a Bee City through Bee City Canada;
 - .4 Permitting the raising of backyard hens on residential properties to enable people to collect eggs; and
 - .5 Permitting residents to establish apiaries on residential properties.

10. Social Needs

Having key social needs met in Ashcroft to the largest extent possible is critical to increasing the quality of life in the community and attracting and retaining residents. Addressing access to basic social needs provides a foundation for community building, participation, and individual well-being. The community has long been concerned about access to social services, particularly healthcare, as the level of service at the Ashcroft Hospital and Community Care Centre has gradually declined. Maintaining and enhancing key social services to ensure that needs are met within the community will be critical moving forward.

10.1. Goals

It is the goal of Council to:

- .1 Develop a community where all ages can thrive and meet most of their social needs.

10.2. Policies

It is the policy of Council to:

- .1 Collaborate on the strengthening of health and wellness in Ashcroft by:
 - .1 Lobbying Interior Health to recruit more health practitioners to the community, such as doctors, nurse practitioners and full-time paramedics, and to maintain and enhance levels of service;
 - .2 Encouraging community partners in their efforts to provide innovative models of healthcare in Ashcroft;
 - .3 Lobbying for long-term care facilities in the community;
 - .4 Working with other communities within the region to attract specialized medical practitioners on a rotational basis;
 - .5 Encouraging the development of a community kitchen program to teach people how to prepare nutritious meals and safely preserve food;
 - .6 Encouraging house and building owners to test for radon;
 - .7 Investigating the development of a program to encourage the switch out of old wood fire places and stoves for more efficient wood or natural gas stoves or fireplaces that meet appropriate standards;
 - .8 Encouraging the expansion of the volunteer driver program to assist those in need in the community; and
 - .9 Encouraging the development of a daycare in Ashcroft that could offer permanent and temporary child minding services.

11. Housing

Across BC communities are confronting significant challenges to providing affordable market and rental housing to residents and Ashcroft is no exception. There is a growing need for a variety of housing options including seniors, rental housing for individuals and families moving to the community, short-term rental housing for labourers, and market housing. Communities are looking for innovative solutions to the housing shortage, including secondary suites, tiny houses, and carriage houses. Going forward, both the community and Council have a role to play in addressing housing concerns and looking for answers.

11.1. Goals

It is the goal of Council to:

- .1 Enable the diversification of the housing stock within Ashcroft to meet a greater range of housing needs.

11.2. Policies and Actions

It is the policy of Council to:

- .1 Enable the diversification of the housing stock by:
 - .1 Encouraging the development of a housing co-operative in Ashcroft to provide affordable housing;
 - .2 Encouraging the development of rental housing through the provision of secondary suites, carriage suites, and rehabilitation of existing multi-family homes and new multi-family homes;
 - .3 Encouraging the rehabilitation and renovation of existing houses in Ashcroft where applicable;
 - .4 Supporting the development of housing to accommodate temporary and permanent workers;
 - .5 Encouraging the development of additional housing appropriate for seniors' independent living;
 - .6 Considering how to market the Village reserve land for residential development;
 - .7 Reviewing lots that have been previously identified for a school site and firehall in the Mesa Vista neighbourhood to determine if houses could be constructed there;
 - .8 Working with BC Housing to provide affordable housing in Ashcroft;
 - .9 Encouraging new houses to be constructed to meet visitability¹³ guidelines that includes a bathroom and visiting areas on the ground floor of a house and a level entry that enables people with reduced mobility more ability to enter and visit homes; and
 - .10 Lobbying Interior Health for more assisted care units in Ashcroft.

¹³ According to the Whole Building Design Guide, *Visitability* refers to an affordable, sustainable, and inclusive design approach for integrating a few core accessibility features as a routine construction practice into all newly built homes. These features allow the home to be visited by relatives, friends, and others who may have disabilities, accommodate short term occupancy by people with disabilities, and facilitate additional adaptations that may be needed by an individual.



PART
3

Land Use Plan

Part 3 of the Official Community Plan is focused on the development of goals and policies relating to land use in specific areas of the community.

12. General Policies

General land use policies apply to the use of land regardless of its land use designation. It is the policy of Council to:

- .1 Encourage infill development where feasible;
- .2 Continue to protect environmentally sensitive areas and ensure that future development follows all relevant environmental guidelines;
- .3 Ensure that development in Ashcroft is compatible with environmental conservation of wildlife and waterways, and avoids negatively impacting air quality in residential areas;
- .4 Ensure that any development taking place on unstable soils be proven to be safe and feasible by a professional geoscientist in good standing with the Engineers and Geoscientists British Columbia (EGBC);
- .5 Preserve Ashcroft's history where possible and promote the historic character of the community;
- .6 Support the Provincial Archeological Branch in its initiatives by:
 - .1 Encouraging developers to undertake archeological reviews of their properties as required by Provincial legislation; and
 - .2 Undertaking archeological overview assessments for all developments led by the Village, such as for infrastructure development projects.



13. Residential

13.1. Background

Ashcroft's key residential areas are located in North Ashcroft, Downtown Ashcroft, and Mesa Vista. There currently exists a mix of housing types, from single detached dwellings on a range of lot sizes, to row housing and duplexes, to low-rise apartment buildings. Currently there is potential for a significant number of new lots to be created on developable land formerly designated for public and institutional uses in the Mesa Vista area. This would enable the development of more than 25 single family lots and if medium density residential use was added, this would help address projected growth in the next five years.

13.2. Goals

It is the goal of Council to:

- .1 Ensure that there is sufficient land designated for residential development to meet the needs for the next 20 years; and
- .2 Support the development of a range of residential land uses and housing types including multi-family and single-family houses.

13.3. Rural Residential

It is the policy of Council to:

- .1 Direct rural residential uses to those areas designated Rural Residential on **Schedule B**;
- .2 Permit modular homes¹⁴ built within the last 10 years to be located in Rural Residential areas provided that they meet Canadian Standard Association A-277 standards;
- .3 Require a minimum lot size of 3000 m² in areas designated Rural Residential;
- .4 Ensure that lots within the Rural Residential area are connected to the community water system;
- .5 Ensure that homes within Rural Residential have an on-site sewerage system and encourage homeowners to maintain their system on an ongoing basis through education initiatives;
- .6 Consider options to develop acreages of 1 – 5 acres in appropriate locations;
- .7 Permit the development of secondary suites within single detached houses;
- .8 Permit the development of detached suites on appropriately sized lots; and
- .9 Permit home-based businesses and industries which are incidental to the home while encouraging businesses to relocate to commercial areas.

¹⁴ **Modular Home** is a manufactured dwelling unit constructed on site from factory assembled modules. These homes have a CSA A277 label to show that they were built in a certified factory and must meet local bylaws and the BC Building Code.

13.4. Low Density Residential

It is the policy of Council to:

- .1 Direct low density residential development to those areas designated for Low Density Residential on **Schedule B**;
- .2 Encourage developers to include space for storage of recreational vehicles, boats, trailers, and similar equipment in new residential developments;
- .3 Encourage single detached dwellings as the primary housing type;
- .4 Permit the development of secondary suites within single detached homes;
- .5 Permit the development of detached suites where lot sizes are appropriate;
- .6 Permit home-based businesses which are incidental to the home while encouraging businesses to relocate to the downtown;
- .7 Permit the establishment of daycares, both within homes and as stand-alone uses;
- .8 Permit the installation of manufactured homes¹⁵;
- .9 Permit modular homes built within the last 10 years to be located in a Low Density Residential area provided that they meet Canadian Standard Association A-277 standards;
- .10 Direct single wide mobile homes to existing mobile home parks and mobile home subdivisions;
- .11 Consider designating new areas for mobile home parks based on the following criteria:
 - .1 Location on the periphery of a Low Density Residential area;
 - .2 Location on a street with higher traffic capacity; and
 - .3 Any other criteria that Council deems appropriate.
- .12 Consider options for developing more residential lots in the Mesa Vista area on parcels previously allocated for community uses;
- .13 Consider allowing duplexes, triplexes, and fourplexes in the Low Density Residential designated areas. The following criteria will be applied to applications for these developments:
 - .1 Location on a street with higher traffic capacity; and
 - .2 Location in proximity to parks and institutional uses.

¹⁵ **Manufactured Home** means a mobile home or a modular home used as a dwelling unit.

Mobile Home means a factory constructed dwelling unit designed to be towed from site to site and subject to Canadian Standards Association (CSA) Z240 Mobile Home Series of Standards. CSA Z240 Standards are specifically for homes that may be moved from one home site to another.

Modular Home is a dwelling unit constructed on site from factory assembled modules. These homes have a CSA A277 label to show that they were built in a certified factory and must meet local bylaws and the BC Building Code.

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- .14 Allow places of worship within Low Density Residential designated areas;
- .15 Encourage owners of heritage homes to maintain the heritage aesthetic;
- .16 Encourage homeowners to rehabilitate and renovate aging homes; and
- .17 Ensure that all residential lots are connected to the community water system and sewer system.

13.5. Medium Density Residential

It is the policy of Council to:

- .1 Direct medium density residential uses to those areas designated as Medium Density Residential on **Schedule B**;
- .2 Allow the development of multi-family homes including row houses and apartments;
- .3 Direct new multi-family development towards Downtown Ashcroft; and
- .4 Allow a maximum density of 75 units per hectare.



14. Downtown Ashcroft

14.1. Background

Downtown Ashcroft continues to be a key part of the village's economic vibrancy and social life. A number of restaurants, cafes, retail stores, and services such as Community Futures, Interior Savings, and the Post Office are located along Railway Avenue. Institutional establishments such as the library and Village Office are in the Downtown as well, along with important social gathering places such as the Legion, the curling club, and Drylands Arena. In recent years, small scale industrial operations have adapted buildings in Downtown Ashcroft to suit their needs which has added more diversity to the businesses in the area. Many services and businesses have been in their location for over 15 years, and continue to be operated by the same owner or have successfully changed hands during that time.

Downtown Ashcroft, with its historical buildings, is also a key attraction for visitors to the area. There are tourism accommodations located in Downtown Ashcroft; this provides visitors with easy access to a walkable Downtown area. There is much potential to build upon the historical character and community spirit of the area.

In general, the Downtown commercial area encompasses Railway Avenue between the riverfront and Eighth Street; Brink Street between First and Fifth Streets; and the east side of Railway Avenue between Eighth Street and the cemetery. The area of the Downtown which is principally residential is predominantly a low-density area with single detached dwellings and some medium density townhouse and apartment units. This residential area in the Downtown lies between the riverfront and the central commercial area of the Village. The vision for this area is that, over time, multiple family development, including housing for senior citizens, will be interspersed with single detached dwellings and heritage homes, thereby creating a range of housing opportunities.

14.2. Goals

It is the goal of Council to:

- .1 Consciously blend commercial with residential and compatible industrial uses to maintain the downtown as a livable and vibrant place.

14.3. Downtown Area

It is the policy of Council to:

- .1 Direct commercial and mixed uses to the land use area designated Downtown Ashcroft on **Schedule B**;
- .2 Recognize Downtown Ashcroft as the retail and commercial centre of the community;
- .3 Permit the development of a mix of community, commercial, residential, and institutional uses within Downtown Ashcroft;
- .4 Consider allowing a greater range of businesses in Downtown Ashcroft that could include light industrial uses where the character of the exterior of the building matches the historical look of existing buildings;

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- .5 Encourage commercial uses to have reduced setbacks on the streetfront where a sidewalk exists. In cases where sidewalks do not exist, ensure that there is a landscaped buffer between the building and the street;
- .6 Encourage off-street parking behind storefronts or in landscaped, communal parking facilities;
- .7 Encourage the incorporation of late 1800s/early 1900s architecture into new and existing development;
- .8 Permit single detached and multiple family dwellings to a density not exceeding 75 dwelling units per hectare;
- .9 Permit residential dwellings on parcels adjacent to Railway Avenue between First and Sixth Streets only if they are located above or behind a ground floor non-residential use;
- .10 Consider developing a community heritage registry to identify heritage buildings in the Downtown Ashcroft area;
- .11 Work toward installing attractive gateway signage and landscaping at the entrances to Downtown Ashcroft;
- .12 Continue to promote the development of Downtown Ashcroft's artistic character;
- .13 Consider opportunities to create an outdoor public space downtown, such as a small plaza or garden, to encourage people to use the area to meet socially and stay for longer in the downtown;
- .14 Review the feasibility of a downtown revitalization tax exemption for new development in the downtown; and
- .15 Encourage the development of a downtown revitalization/beautification committee.



15. Industrial Areas

15.1. Background

The Village of Ashcroft has a diverse industrial sector. An important part of it is closely tied to the railways: a manufacturing plant produces ties for the railway while the Ashcroft Terminal performs transloading services between the Canadian Pacific (CP) and Canadian National (CN) rail lines. Access to rail and highway transport is also important for other manufacturers in the area.

Going forward, Ashcroft aims to continue to support and enhance the industrial sector and at the same time be mindful of the potential need to attract workers and accommodate them in rental or market housing.

Ashcroft's three industrial areas are: along the CN rail line and Hollis Road in North Ashcroft, northeast of Downtown Ashcroft in the Ashcroft Terminal area, and an area south of Downtown Ashcroft currently used by Highland Valley Copper for CP Rail purposes.

15.2. Goals

It is the goal of Council to:

- .1 Consider how to leverage access to rail, highway, and fibre optics to attract new businesses.

15.3. Policies

It is the policy of Council to:

- .1 Direct industrial uses to those areas designated Industrial on **Schedule B**;
- .2 Direct light industrial and service commercial to the area designated as Industrial in North Ashcroft;
- .3 Direct heavy industrial uses to the land use area designated as Industrial in northeast Ashcroft;
- .4 Direct public works yards and related maintenance facilities to those areas designated as Industrial;
- .5 Encourage the further development of the Ashcroft Terminal lands as a site of future industrial development;
- .6 Reinforce industrial areas as being primarily for industrial purposes with residential as a secondary purpose;
- .7 Direct sand and gravel extraction activities to those areas designated for sand and gravel extraction on **Schedule B**. This policy is contingent on the approval of provincial agencies, including the Agricultural Land Commission;
- .8 Ensure industrial development complies with applicable environmental standards;
- .9 Consider the impact of air, noise, and light pollution on local residents when evaluating any industrial development application;
- .10 Ensure that industrial facilities are connected to the community water system and sewer system, except for the industrial area northeast of Downtown Ashcroft;
- .11 Promote industrial development that is compatible with current servicing infrastructure; and
- .12 Ensure adequate screening of industrial uses from adjacent non-industrial uses.

16. Community Uses

16.1. Background

Community uses are critical to attracting and encouraging people to stay in Ashcroft. Community uses are located throughout Ashcroft. Public and institutional facilities include the cemetery, the Chinese cemetery, the Village Office, the Thompson Nicola Regional District Library, numerous places of worship in the Downtown, the Royal Canadian Mounted Police (RCMP) detachment, the hospital, the office of School District 74 and Desert Sands Community School in North Ashcroft. The previous site of Ashcroft Elementary, located in North Ashcroft, is currently being repurposed primarily as a recreation and social centre called the Ashcroft HUB.

Important recreational focal points in Ashcroft are its parks and recreational facilities. There is a mix of park space, playgrounds, sports fields, and facilities that include the outdoor pool, Drylands Arena, and the curling rink. These parks and recreational facilities support a high quality of life in Ashcroft and also have regional importance. Much of the community continues to be interested in the development of trail systems between the neighbourhoods and in creating better access points to riverine areas and the river.

16.2. Goals

It is the goal of Council to:

- .1 Maintain and enhance public and institutional facilities; and
- .2 Maintain and enhance parks and recreational uses in a manner that is appropriate for a small town and meets the needs of people of all ages.

16.3. Policies

It is the policy of Council to:

- .1 Direct all community uses to those areas designated for Community Use on **Schedule B**;
- .2 Maintain and enhance public and institutional facilities by:
 - .1 Determining options to use surplus public land for key community needs such as seniors' housing;
 - .2 Encouraging public and institutional uses to be easily accessible to pedestrians, seniors, and youth through the use of trails, sidewalks and accessible parking; and
 - .3 Working with government agencies to determine the best use of vacant land located adjacent to the RCMP detachment and the hospital.
- .3 Maintain and enhance parks and recreational facilities by:
 - .1 Leveraging parks and recreational facilities to build intergenerational connections in Ashcroft;
 - .2 Encouraging parks and recreational facilities to be easily accessible to pedestrians, seniors, and youth;
 - .3 Requiring the payment of cash in lieu of parkland, while prioritizing maintaining existing parkland;

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- .4 Ensuring that proposed investments into existing and new facilities have a business case that supports the investment;
- .5 Supporting infrastructure improvements to the Drylands Arena, particularly to the dressing rooms, should funding or financial resources become available;
- .6 Continuing to encourage the work of community organizations in beautification of downtown and in Village parks;
- .7 Reviewing the feasibility of developing a dog park, preferably on existing park land;
- .8 Reviewing options to revitalize the curling rink facility;
- .9 Reviewing the feasibility of developing a riverfront trail and/or improving public access points to the Thompson River, including whether it is feasible to provide a public access to the slough;
- .10 Encouraging the continued operation of arts and recreational programs at the Ashcroft HUB;
- .11 Determining the feasibility of developing recreational facilities such as a BMX park, lawn bowling facility, waterpark, pickleball court, and/or improvements to the skateboard park;
- .12 Reviewing the feasibility of upgrading the pool for year-round use, comparing capital and operating costs with potential usage, as well as reviewing the re-opening of the hot tub;
- .13 Reviewing fiscally responsible opportunities to provide cemetery space;
- .14 Working with the community to formalize trails throughout the community; and
- .15 Working with community partners to develop a demonstration garden for native vegetation and a community garden.

17. Agriculture

17.1. Background

Agriculture has traditionally been a key part of Ashcroft's economy. Whereas in the past there were tomato production and canning facilities in Ashcroft, today activity centres around local agricultural production; Desert Hills, which continues to expand its production; and nearby ranching. The Ashcroft Ranch area southwest of Downtown Ashcroft is leased to Blue Goose Cattle Company for this purpose.

It is important to the community to continue to support agricultural operations in the area. There are significant areas of agricultural production in North Ashcroft associated with Desert Hills which are part of the Agricultural Land Reserve (ALR). The majority of the Ashcroft Ranch is also within the ALR. Additional ALR areas are found along the southern edge of the Village boundary on the east side of the Thompson River, as well within the southern portion of northeast Ashcroft adjacent to the Ashcroft Terminal lands.

17.2. Goals

It is the goal of Council to:

- .1 Support the use of agricultural land to contribute to local food security and build economic development;
- .2 Protect agricultural lands; and
- .3 Ensure the *Agricultural Land Commission Act* and the *Local Government Act* are applied in regard to any land use or building application submitted in the Agricultural Land Reserve.

17.3. Policies

It is the policy of Council to:

- .1 Direct agricultural uses to those areas designated Agriculture on **Schedule B**;
- .2 Support the mandate of the Agricultural Land Commission concerning lands within the ALR within Ashcroft's boundaries by:
 - .1 Supporting applications for the exclusion of lands from the ALR only if:
 - .1 It can be demonstrated that soil conditions are not suitable for agriculture,
 - .2 The parcels clearly have a history of non-agricultural use that pre-dates the ALR; and
 - .3 The land has already been identified for exclusion by the Agricultural Land Commission.
 - .2 Supporting the Agricultural Land Commission's objective of retaining agricultural lands and consolidating them into large parcels to maintain their viability for agricultural use; and
 - .3 Ensuring that the provisions of the *Agricultural Land Commission Act* and those of the *Local Government Act* are taken into account in any land use or building application being reviewed by the Village for property that is located within the ALR.
- .4 Use agricultural land to support economic development and provision of locally sourced food by:

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- .1 Encouraging the maintenance and enhancement of agriculture in those areas designated as Agriculture in this Official Community Plan;
- .2 Reviewing opportunities to leverage agriculture in Ashcroft to create jobs and generate spin-off activities such as value-added processing and tourism;
- .3 Encouraging individuals to establish agricultural operations in and around Ashcroft;
- .4 Encouraging the utilization of agricultural lands and the diversification of crops to include wineries, hops farms, and produce; and
- .5 Encouraging the use of underutilized agricultural land.

18. Open Space

18.1. Background

Ashcroft's dramatic and unique landscape is an area rich in wildlife, both in the rivers and on the land. The landscape today is characterized by terraces and river valleys which are the result of the glacial deposits of sediments. The Bonaparte and the Thompson River continue to erode this sediment, and what remains are steep slopes, areas of unstable soils, riparian areas, and wildfire hazards. These lands will have development limited to nature trails.

18.2. Goals

It is the goal of Council to:

- .1 Set aside land as Open Space areas to promote public safety, provide recreation opportunities, and conserve habitat that contributes toward biodiversity;
- .2 Protect areas within or adjacent to Village boundaries which are environmentally sensitive; and
- .3 Promote public safety and minimize damage to property by exercising caution in areas exhibiting hazardous or potentially hazardous conditions.

18.3. Open Space Areas

It is the policy of Council to:

- .1 Designate as Open Space those areas shown on **Schedule B**;
- .2 Prohibit development of buildings on Open Space areas, unless deemed necessary by the Village for community infrastructure purposes or deemed safe by a qualified professional;
- .3 Support Open Space areas as a means of promoting biodiversity and enriching the urban environment;
- .4 Support passive recreational use such as walking, hiking, fishing, and nature observation in Open Space areas; and
- .5 Supporting the conversion of lands between the CN Railway Right-of-Way and the Thompson River to Open Space use. Mandated setbacks from front parcel lines set out in the Village's Zoning Bylaw make development of these parcels very difficult. Consequently, Council will encourage Open Space use on these parcels over time.

19. Infrastructure

19.1. Background

As described in **Section 3.6** Ashcroft has well developed infrastructure including a water and sewer system and a road network. A key challenge for small communities is the ability to financially support infrastructure operations, maintenance, upgrades and expansion, particularly when faced with a potentially declining tax base. Despite this challenge, the Village has upgraded both the water and sewage treatment plants. Moving forward, it is vital that the Village assess future investments in terms of the initial investment required, the ongoing cost of operations, and present and future need.

19.2. Goals

It is the goal of Council to:

- .1 Ensure that infrastructure is developed and maintained in a fiscally responsible and sustainable manner; and
- .2 Ensure that municipal infrastructure meets the needs of residents and businesses today and into the future.

19.3. Policies and Actions

It is the policy of Council to:

- .1 Ensure that infrastructure is developed and maintained in a fiscally responsible manner by:
 - .1 Addressing the infrastructure deficit in Ashcroft by continuing to invest in asset management and seeking funding from senior government agencies to address deficits; and
 - .2 Striving to improve the community's understanding of the costs associated with infrastructure and how it needs to be managed.
- .2 Ensure that the water and sewer systems meet community needs by:
 - .1 Maintaining and operating the public water and sewer system as shown on **Schedule E**;
 - .2 Twinning the North Ashcroft water reservoir in order to provide fire protection in this area;
 - .3 Replacing Lift Station # 1 to ensure the proper operation of the sewer system;
 - .4 Reviewing water leakage in the community water system to identify and fix potential leaks;
 - .5 Undertaking water audits of community facilities, parks and key commercial businesses to determine how water is used in the community;
 - .6 Undertaking a municipal water and energy study to review usage of water and energy in the Village;
 - .7 Working with key partners and stakeholders in protecting the Thompson River, Ashcroft's principal water source;
 - .8 Encouraging water conservation in homes, businesses and community facilities to reduce costs of providing water and protecting water quantity for ecological needs;

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- .9 Encouraging new and existing developments to incorporate xeriscaping into site designs; and
 - .10 Considering the implementation of water meters.
- .3 Ensure that storm drainage meets community needs by:
- .1 Maintaining the public storm drainage system as shown on **Schedule E**, the Public Utilities Map;
 - .2 Requiring all development to manage stormwater in a manner which provides adequate disposal and minimizes impact to adjacent properties;
 - .3 Discouraging development which has a negative impact on existing drainage channels and gullies;
 - .4 Encouraging rain water reuse in residential, commercial and community land uses;
 - .5 Improving the storm drainage system in North Ashcroft; and
 - .6 Considering alternative approaches to stormwater management, including the use of bioswales and use of rain barrels.
- .4 Ensure that the transportation network meets existing and emerging needs by:
- .1 Designating major roads as shown on **Schedule F**.
 - .2 Upgrading sidewalks in town and considering opportunities to add new sidewalks or other types of pedestrian connections where feasible;
 - .3 Continuing to work with the Ministry of Transportation and Infrastructure to establish a vehicle and/or pedestrian overpass or underpass for Highway 97C crossing the Canadian Pacific Railway in order to provide continuous emergency services to the Mesa Vista area;
 - .4 Investigating the integration of traffic calming measures, such as narrowing roadways, installing roundabouts, or constructing speed bumps in all neighbourhoods in order to reduce speeds and create a more comfortable network for pedestrians;
 - .5 Implementing a maximum traffic speed of 30 km/hr on all non-arterial roads;
 - .6 Ensuring that streets are not used for recreational vehicle storage in order to enable safer navigation for pedestrians; and
 - .7 Working with the Desert Hill Community School and School District # 74 (Gold Trail) to develop a Safe Routes to School program.

20. Development Permit Area Guidelines

Pursuant to the *Local Government Act* (s.488(1)), Council may designate Development Permit Areas. These may be established under the following conditions:

- .1 Protection of the natural environment, its ecosystems and biological diversity;
- .2 Protection of development from hazardous conditions;
- .3 Protection of farming;
- .4 Revitalization of an area in which a commercial use is permitted;
- .5 Establishment of goals for the form and character of intensive residential development;
- .6 Establishment of objectives for the form and character of commercial, industrial or multi-family residential development;
- .7 In relation to an area in a resort region, establishment of objectives for the form and character of development in the resort region;
- .8 Establishment of objectives to promote energy conservation;
- .9 Establishment of objectives to promote water conservation; and
- .10 Establishment of objectives to promote the reduction of greenhouse gas emissions.

This Plan contains four Development Permit Areas:

<p>DPA #1 Streamside Protection:</p>	<p>Riparian areas within the Village have been designated to protect fish habitat.</p>
<p>DPA #2 Hazardous Slopes and Soils:</p>	<p>These areas are considered hazardous because of steeply sloping terrain as well as the presence of several gullies and unstable soils.</p>
<p>DPA #3 Downtown Multiple Family Residential Area:</p>	<p>The form and character of future multi-family residential development in the downtown.</p>
<p>DPA #4 Downtown Commercial/Mixed Use Area:</p>	<p>This Development Permit Area has been established to implement design guidelines to reflect the architecture of the turn-of-the-century Victorian frontier period, and to implement parking and loading requirements that support the revitalization of the downtown commercial area.</p>

20.1. Development Permit Area No. 1 – Streamside Protection

20.1.1 Authorization

Lands adjacent to streams are designated as a Development Permit Area pursuant to Section 488(1)(a) of the *Local Government Act* for the protection of the natural environment, its ecosystems and biological diversity.

20.1.2 Designated Area

Development Permit Area No. 1 consists of all the riparian assessment areas within the Village of Ashcroft as illustrated on **Schedule D**.

20.1.3 Justification

Development Permit Area No. 1 seeks to protect riparian areas to support fish life processes. Council seeks to protect this resource from any potentially negative impacts associated with development.

20.1.4 Development Permit Triggers

The *Local Government Act* imposes development permit requirements in development permit areas. Unless exempted by 20.1.5, a development permit must be obtained when any of the following types of development activity occur in Development Permit Area No. 1, in accordance with Section 489 of the *Local Government Act*:

- a) Subdivision of land;
- b) Construction, addition or alteration of a building or other structure; or
- c) Alteration of land.

20.1.5 Exemptions

In Development Permit Area No. 1, a development permit is not required for the following:

- a) Development Permit Area No. 1 does not apply to the reconstruction or repair of a permanent structure if the structure remains on its existing foundation.
- b) Development Permit Area No. 1 does not apply to agricultural, mining, or forestry related development.

20.1.6 Guidelines

Development within Development Permit Area No. 1 shall be permitted only if:

- a) A qualified environmental professional carries out an assessment of the proposed development in accordance with the *Riparian Areas Regulation* of the *Fish Protection Act*, and shows that the proposed development protects riparian areas as required by the *Riparian Areas Regulation*, and a notification is received from the appropriate federal and provincial government ministries stating that they have been notified of the development proposal as per the requirements of the *Riparian Areas Regulation*; or
- b) The relevant federal government ministries or a regulation under the *Fisheries Act* (Canada) authorizes the harmful alteration, disruption, or destruction of natural features,

functions and conditions that support fish life processes in the riparian assessment area that would result from the implementation of the development proposal.

20.2. Development Permit Area No. 2 – Hazardous Slopes and Soils

20.2.1 Authorization

Development Permit Area No. 2 is designated pursuant to *Local Government Act* Section 488(1)(b) [protection of development from hazardous conditions].

20.2.2 Designated Area

Development Permit Area No. 2 is the area shown as Hazardous Slopes and Soils on **Schedule D**.

20.2.3 Justification

Development Permit Area No. 2 seeks to protect development from hazardous conditions such as steeply sloping terrain, intermittent stream gullies with sloughing embankments, and unstable slopes prone to topsoil movement. Evidence of these conditions is found from air photo interpretation, topographic mapping analysis, field reconnaissance, knowledge of past history and the October 25, 1996 Golder Associates Geotechnical/Natural Hazard Assessment. Areas also include steep slopes with grades of 30% or greater, areas of soil subsidence, rock fall, land slip or erosion hazards which are known or suspected. Council will, however, consider permitting development on steep slopes or areas subject to soil subsidence, rock fall, land slips or erosion hazards, if the development or subdivision proponent provides a report from a professional engineer experienced in geotechnical engineering setting out how the area can be developed safely.

20.2.4 Development Permit Triggers

The *Local Government Act* imposes development permit requirements in development permit areas. Unless exempted by 20.2.5, a development permit must be obtained when any of the following types of development activity occur in Development Permit Area No. 2, in accordance with Section 489 of the *Local Government Act*:

- a) Subdivision of land;
- b) Construction, addition or alteration of a building or other structure; or
- c) Alteration of land.

20.2.5 Exemptions

In Development Permit Area No. 2, a development permit is not required for the following:

- a) The alteration of a building that is limited to the addition, replacement or alteration of doors, windows, building trim, awnings, or roofs;
- b) Interior alterations to buildings;
- c) The construction of new buildings or structures less than 10 square metres in area;
- d) The erection of a sign or fence;
- e) The placement of temporary construction site offices, structures used for short-term special events and emergency facilities; or

- f) The consolidation of a lot or road widening.

20.2.6 Guidelines

The following Development Permit Guidelines apply in Development Permit Area No. 2:

- a) Areas of steep slopes are identified as those having a natural slope of greater than 30% for more than 1.5 metres and/or may pose a potential for landslide or other geotechnical hazard;
- b) Any future development, including but not limited to the altering of land and soil, the erection and placement of buildings and the installation of in-ground sanitary or storm sewer systems, shall ensure that erosion mitigation and slope stabilization measures are implemented;
- c) Steep slope lands shall preferably be maintained in their natural state. All persons desiring to alter or affect lands within the designated area will be required to submit to the Village of Ashcroft a report certified by a geoscientist registered with EGBC to assist the Village in determining what conditions or requirements it will impose in the permit.
- d) Existing vegetation shall be maintained in order to avoid erosion, slumping and mass wasting. Access improvements on a steep slope such as roadways, pathways and trails shall be located and constructed so as not to disturb the slope or natural drainage;
- e) Buildings and structures shall be sited in accordance with building setbacks and other requirements as determined by a geoscientist registered with EGBC; and
- f) Provisions shall be made for the disposal of surface run-off and stormwater drainage to be diverted away from hazard slope areas subject to sloughing or erosion.

20.3. Development Permit Area No. 3 – Downtown Multiple Family Residential Area

20.3.1 Authorization

Development Permit Area No. 3 is designated pursuant to *Local Government Act* Section 488(1)(e) [establishment of objectives for the form and character of intensive residential development].

20.3.2 Designated Area

The Downtown Multiple Family Residential Development Permit Area is shown on **Schedule D**, the Development Permit Area Map as Development Permit Area No. 3.

20.3.3 Justification

Development Permit Area No. 3 seeks to achieve a high standard of quality for multi-family residential development. Downtown Ashcroft is in transition and new multi-family residential development should be sensitive to existing residential and commercial buildings. The Development Permit Area also seeks to create a greater sense of community by promoting good form and character.

20.3.4 Development Permit Triggers

The *Local Government Act* imposes development permit requirements in development permit areas. Unless exempted by 20.3.5, a development permit must be obtained when any of the following types of

development activity occur in Development Permit Area No. 3, in accordance with Section 489 of the *Local Government Act*:

- a) Subdivision of land;
- b) Construction, addition or alteration of a building or other structure; or
- c) Alteration of land.

20.3.5 Exemptions

Development Permit Area No. 3 does not apply to any non-multi-family residential development, which includes single family dwellings and duplexes, commercial development, and institutional development.

In Development Permit Area No. 3, a development permit is not required for the following:

- a) The alteration of a building that is limited to the addition, replacement or alteration of doors, windows, building trim, awnings, or roofs;
- b) Interior alterations to buildings;
- c) The construction of new buildings or structures less than 10 square metres in area;
- d) The erection of a sign or fence;
- e) The placement of temporary construction site offices, structures used for short-term special events and emergency facilities; or
- f) The consolidation of a lot or road widening.

20.3.6 Guidelines

The following Development Permit Guidelines apply in Development Permit Area No. 3:

Site Planning

- a) First storey dwelling units should have individual access to grade to create the impression of a single family area;
- b) If a new multi-family dwelling is adjacent to a parcel containing a single family dwelling, the multi-family dwelling units immediately adjacent to the single family dwelling should be encouraged to not be greater than one storey higher than the adjacent single family dwelling; the multi-family dwelling, could however, be stepped back to a higher height away from the single family dwelling; and
- c) The maximum setback of buildings from the street should be 4 to 6 metres.

Design

- d) The main, common entrance for dwelling units above the first floor should be subtle;
- e) The use of porches, balconies, dormers, bay windows and sub-roofs should be encouraged to break up the massiveness of the structure in order to create the appearance of several smaller structures rather than one large block;
- f) The use of a variety of earth-tone or pastel colours should also be considered to break up the massiveness of the structure; and

- g) The side or back of the multi-family dwelling should be less detailed than the front but should not consist of barren walls.

Landscaping

- h) Development should include xeriscaped areas perpendicular to the street.

Parking

- i) Access to parking should be from a lane and parking should be behind the dwelling units fronting the street.

20.4. Development Permit Area No. 4 – Downtown Commercial/Mixed Use Area

20.4.1 Authorization

Development Permit Area No. 4 is designated pursuant to *Local Government Act* Section 488(1)(d) [revitalization of an area in which a commercial use is permitted].

20.4.2 Designated Area

The Downtown Area is shown on **Schedule D**, the Development Permit Area Map as Development Permit Area No. 4.

20.4.3 Justification

Development of this area in a manner reminiscent of turn-of-the-century Ashcroft is vital to the general social and economic well-being of Ashcroft and the specific revitalization of the downtown commercial district. The Development Permit Area also seeks to support downtown revitalization and to establish objectives for the form and character of commercial development through the implementation of appropriate parking and loading requirements.

20.4.4 Development Permit Triggers

The *Local Government Act* imposes development permit requirements in development permit areas. Unless exempted by 20.4.5, a development permit must be obtained when any of the following types of development activity occur in Development Permit Area No. 4, in accordance with Section 489 of the *Local Government Act*:

- .1 Subdivision of land;
- .2 Construction, addition or alteration of a building or other structure; or
- .3 Alteration of land.

20.4.5 Exemptions

In Development Permit Area No. 4, a development permit is not required for the following:

- .1 The alteration of a building that is limited to the addition, replacement or alteration of doors, windows, building trim, awnings, or roofs as long as these alterations conform with the guidelines of Development Permit Area No. 4;
- .2 Interior alterations to buildings;
- .3 The construction of new buildings or structures less than 10 square metres in area;

Bylaw No. 822, 2018

- .4 The erection of a sign or fence;
- .5 The placement of temporary construction site offices, structures used for short-term special events and emergency facilities; or
- .6 The consolidation of a lot or road widening.

20.4.6 Guidelines

The following Development Permit Guidelines apply in Development Permit Area No. 4:

Design

- a) The design guidelines of Development Permit Area No. 4 are contained in Schedule G, Facade Guidelines for the Downtown of the Village of Ashcroft.

Parking and Loading

- b) Requirements for off-street parking and off-street loading spaces shall be determined on a case by case basis;
- c) Off-street parking and off-street loading requirements may be waived if it is determined that expected parking and loading needs can be accommodated on-street and/or in parking facilities within the vicinity of the development;
- d) In determining the number of off-street parking and off-street loading spaces required, the following shall be considered:
 - i. The expected demand for parking and loading generated by the development;
 - ii. The ability to accommodate parking and loading demand on-street and/or in parking facilities within the vicinity of the development;
- e) Any required off-street parking and off-street loading spaces shall be provided in general accordance with the location and siting requirements and development and maintenance standards for off-street parking and off-street loading facilities contained in the Village's *Zoning Bylaw*.

21. Temporary Use Permits

Council may issue permits for temporary activities, as per **Section 492** of the *Local Government Act*. Temporary commercial uses may include fruit and vegetable stands, while temporary industrial uses may include asphalt or cement batch plants. If a temporary use permit is issued, the maximum period of time for these permits is three years and the permit may be renewed only once for an additional three years provided that the temporary use:

- .1 Is not noxious or undesirable because of smoke, noise, vibration, dirt, glare, odour, radiation, electrical interference or an offensive trade;
- .2 Is compatible with current and/or future uses on adjacent lands;
- .3 Does not have a negative impact on the natural environment;
- .4 Does not create a significant increase in the level or demand for services;
- .5 Does not permanently alter the site upon which it is located, unless this results in value to the site;
- .6 Complies with all the conditions specified by Council in the Temporary Use Permit;
- .7 Mitigates traffic impacts in a manner approved by the Ministry of Transportation and Infrastructure, where applicable; and
- .8 Complies with other Provincial and Federal enactments.

Part 4 summarizes the strategy for implementing the policies and actions outlined in this OCP and suggests potential timelines and responsibilities for implementation.

Action	Relevant Policies	Priority	Leadership	Support
Develop marketing materials for promoting development in Ashcroft and relocation for health professionals	6.2.1.1 6.2.1.2 6.2.1.3 6.2.1.6	High	Village of Ashcroft	
Restart the Chamber of Commerce	6.2.2.2	Moderate	Ashcroft business community	Village of Ashcroft
Develop marketing materials for tourism in Ashcroft	6.2.3.1	Moderate	Gold Country	Village of Ashcroft
Review the feasibility of having a sister city relationship with a Mexican community	6.2.3.6	Low	Village of Ashcroft	
Install visually attractive signs on Highway 1 to encourage motorists to come into Ashcroft	6.2.3.4	Moderate	Village of Ashcroft	Gold Country, Ministry of Transportation and Infrastructure
Develop an agricultural strategy to determine how to leverage agriculture as an economic development initiative	6.2.3 6.2.3	Moderate	Village of Ashcroft	BC Ministry of Agriculture
Support the development of an Urban Tree Program	7.2.1.9	Moderate	Communities in Bloom	Village of Ashcroft
Update the emergency plan for Ashcroft	7.2.3.1 7.2.3.6 7.2.3.7	High	Village of Ashcroft	
Install a centrally located signboard to provide emergency alerts and notices of community events	7.2.3.4	High	Village of Ashcroft	
Develop an emergency access route out of the Mesa Vista neighbourhood	7.2.3.5	Moderate	Village of Ashcroft	
Provide education materials to residents on how they can reduce greenhouse gas emissions	8.2.1.1	Low	Village of Ashcroft	
Consider the adoption of an anti-idling program	8.2.1.2	Low	Village of Ashcroft	

Bylaw No. 822, 2018

Action	Relevant Policies	Priority	Leadership	Support
Review options to enable vehicle electrification by adding an electric vehicle charging station and permitting electric golf carts	8.2.1.3 8.2.1.4	Moderate	Village of Ashcroft	Ministry of Transportation and Infrastructure
Develop a trails master plan to review where trails should be located and how they should be funded	8.2.1.5	Moderate	Village of Ashcroft	
Develop a community energy plan to review options for alternative energy in the community while reviewing potential means of promoting energy efficiency in existing and new buildings	8.2.1.8 8.2.1.9 8.2.1.10 8.2.1.11 8.2.2	Moderate	Village of Ashcroft	
Review the feasibility of a community compost system	8.2.1.15	Low	Village of Ashcroft	
Hire a grant writer to help community organizations obtain grant funding	9.2.1.7	Moderate	Village of Ashcroft	Northern Development Initiative
Develop more community events and activities to bring people together	9.2.1.4 9.2.2.1	Moderate	Community Groups	Village of Ashcroft
Develop a community garden	8.2.1.16 9.2.3.1	Low	Community Groups	Village of Ashcroft
Develop a community food security program	9.2.3.2 9.2.3.3 10.2.1.5	Low	Community Groups	Village of Ashcroft Interior Health
Investigate means of providing affordable housing in Ashcroft	11.2.1.8	Moderate	Village of Ashcroft	BC Housing
Develop a community heritage registry to recognize historical sites and buildings of interest	14.3.10	Low	Village of Ashcroft	Ashcroft Museum
Install gateway signage and landscaping at the entrances to Downtown Ashcroft	14.3.11	Low	Village of Ashcroft	
Review the feasibility of a downtown revitalization tax exemption	14.3.15	Moderate	Village of Ashcroft	
Review the feasibility of developing a dog park	16.3.3.7	High	Village of Ashcroft	
Develop a parks and recreation master plan that will review and prioritize upgrades	16.3.3.4 16.3.3.5	Moderate	Village of Ashcroft	

Bylaw No. 822, 2018

Action	Relevant Policies	Priority	Leadership	Support
to existing facilities and consider the feasibility of developing new facilities including trails in the community	16.3.3.8 16.3.3.9 16.3.3.11 16.3.3.12			
Upgrade Lift Station # 1	19.3.2.3	High	Village of Ashcroft	
Twin the North Ashcroft water reservoir	19.3.2.2	High	Village of Ashcroft	
Review water demand in Ashcroft by examining for possible leaks in the system and reviewing high demand water users	19.3.2.4 19.3.2.5 19.3.2.6	Moderate	Village of Ashcroft	
Review the feasibility of improving the storm drainage system in North Ashcroft	19.3.3.5	Low	Village of Ashcroft	
Upgrade pedestrian connections throughout Ashcroft where feasible and warranted	19.3.4.2	Moderate		
Review opportunities to improve connection across the CP Rail tracks on Highway 97C	19.3.4.3	Moderate	Village of Ashcroft	Ministry of Transportation and Infrastructure CP Rail
Implement a maximum traffic speed of 30 km/h on non-arterial roads	19.3.4.5	Moderate	Village of Ashcroft	
Develop a Safe Routes to School program	19.3.4.7	Moderate	School District # 74	Village of Ashcroft


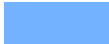





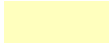

**Schedule B
Land Use Plan**

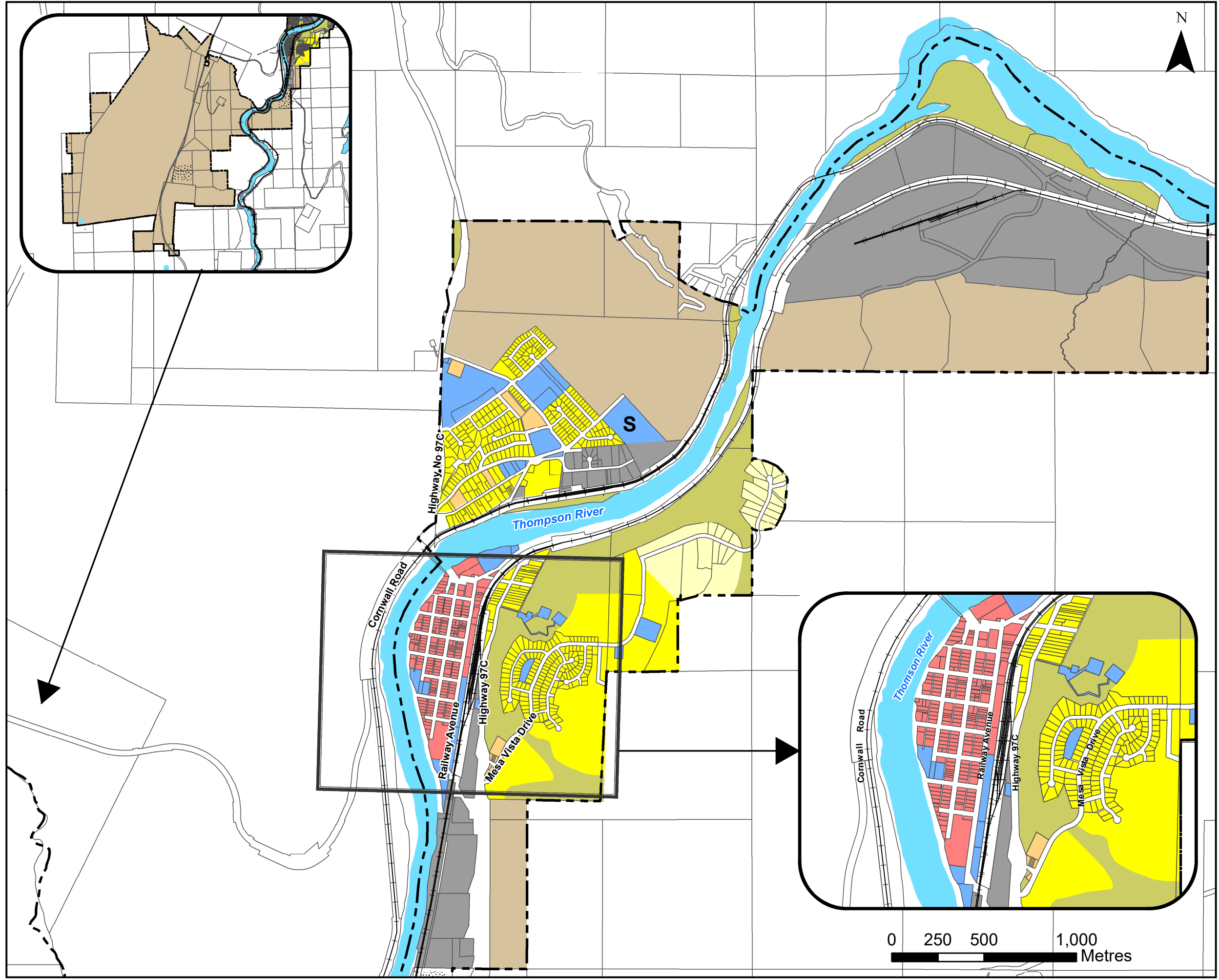


Village of Ashcroft Official Community Plan Schedule B Land Use Plan

Legend

LAND USE

-  AGRICULTURE
-  COMMUNITY USE
-  DOWNTOWN ASHCROFT
-  INDUSTRIAL
-  LOW DENSITY RESIDENTIAL
-  MEDIUM DENSITY RESIDENTIAL
-  OPEN SPACE
-  RURAL RESIDENTIAL
-  SAND AND GRAVEL



SOURCE:
Water and road features obtained through www.geobase.ca website.
Cadastral information obtained through ParcelMap BC.
Municipal Boundary obtained through DataBC.

THE ACCURACY & COMPLETENESS OF INFORMATION SHOWN ON THIS DRAWING IS NOT GUARANTEED. IT WILL BE THE RESPONSIBILITY OF THE USER OF THE INFORMATION SHOWN ON THIS DRAWING TO LOCATE & ESTABLISH THE PRECISE LOCATION OF ALL EXISTING INFORMATION WHETHER SHOWN OR NOT.

Schedule C

Environmentally Sensitive and Hazardous Areas







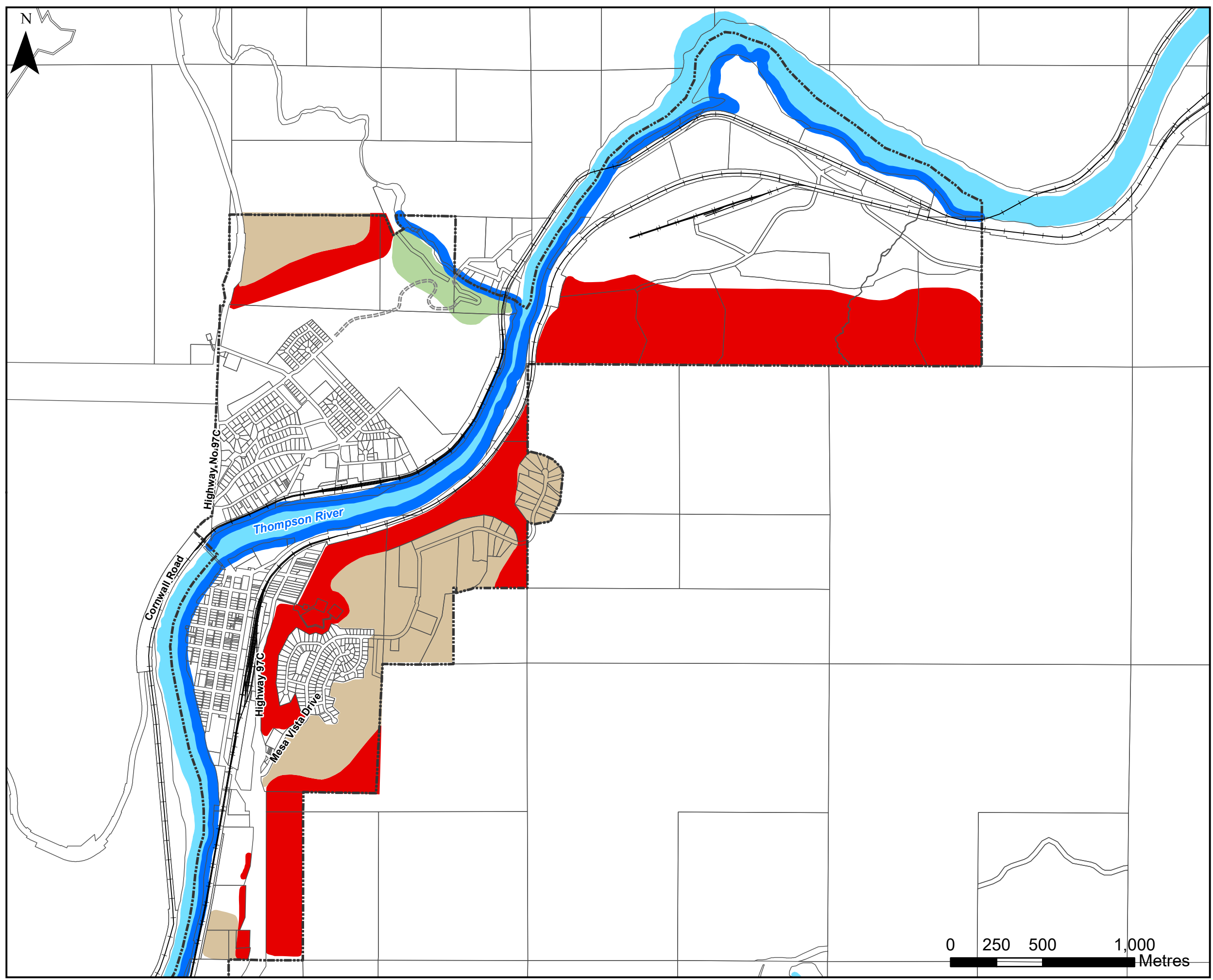
Village of Ashcroft

Official Community Plan Schedule C

Environmentally Sensitive and Hazardous Areas

Legend

-  Floodplain
-  Environmentally Sensitive Areas
-  Hazardous Slopes
-  Unstable Soils



SOURCE:

Water and road features obtained through
www.geobase.ca website.

Cadastral information obtained through ParcelMap BC,
Municipal Boundary provided by DataBC

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THE USER OF THE INFORMATION SHOWN ON THIS DRAWING TO LOCATE
& ESTABLISH THE PRECISE LOCATION OF ALL EXISTING INFORMATION
WHETHER SHOWN OR NOT.

Schedule D
Development Permit Areas







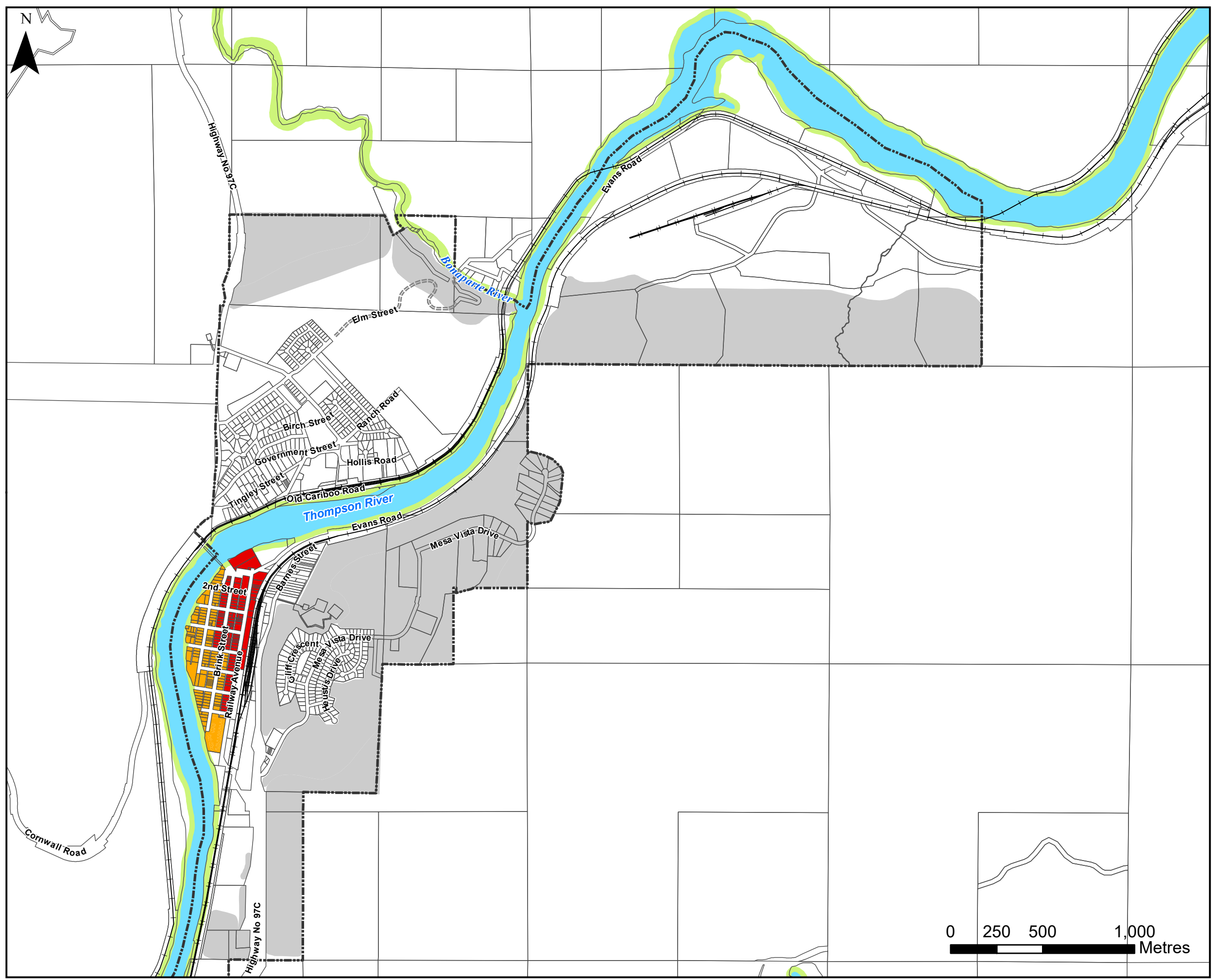
Village of Ashcroft

Official Community Plan Schedule D

Development Permit Areas

Legend

-  DPA #1 - Streamside Protection
-  DPA#2 - Hazardous Slopes and Soils
-  DPA #3 - Downtown Multiple Family Residential Area
-  DPA #4 - Downtown Commercial / Mixed Use Area



SOURCE:

Water and road features obtained through www.geobase.ca website.

Cadastral information obtained through ParcelMap BC, Municipal Boundary obtained through DataBC.

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Schedule E
Public Utilities








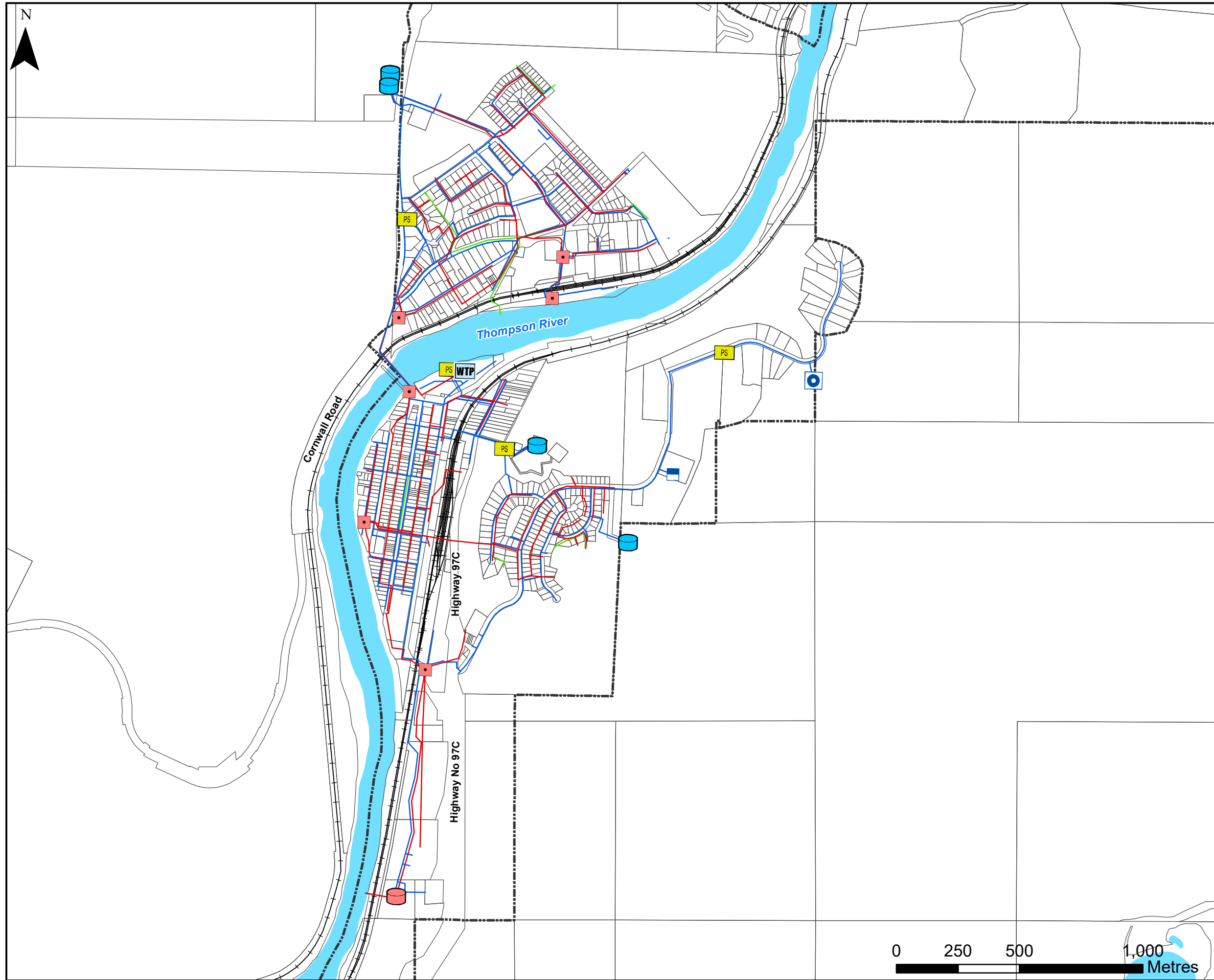
Village of Ashcroft

Official Community Plan Schedule E

Public Utilities

Legend

-  Sanitary
-  Water
-  Storm
-  Water Treatment Plant
-  Reservoir
-  Pump Station
-  Balancing Tank
-  Irrigation Control
-  Waste Water Treatment Plant
-  Lift Station



SOURCE:

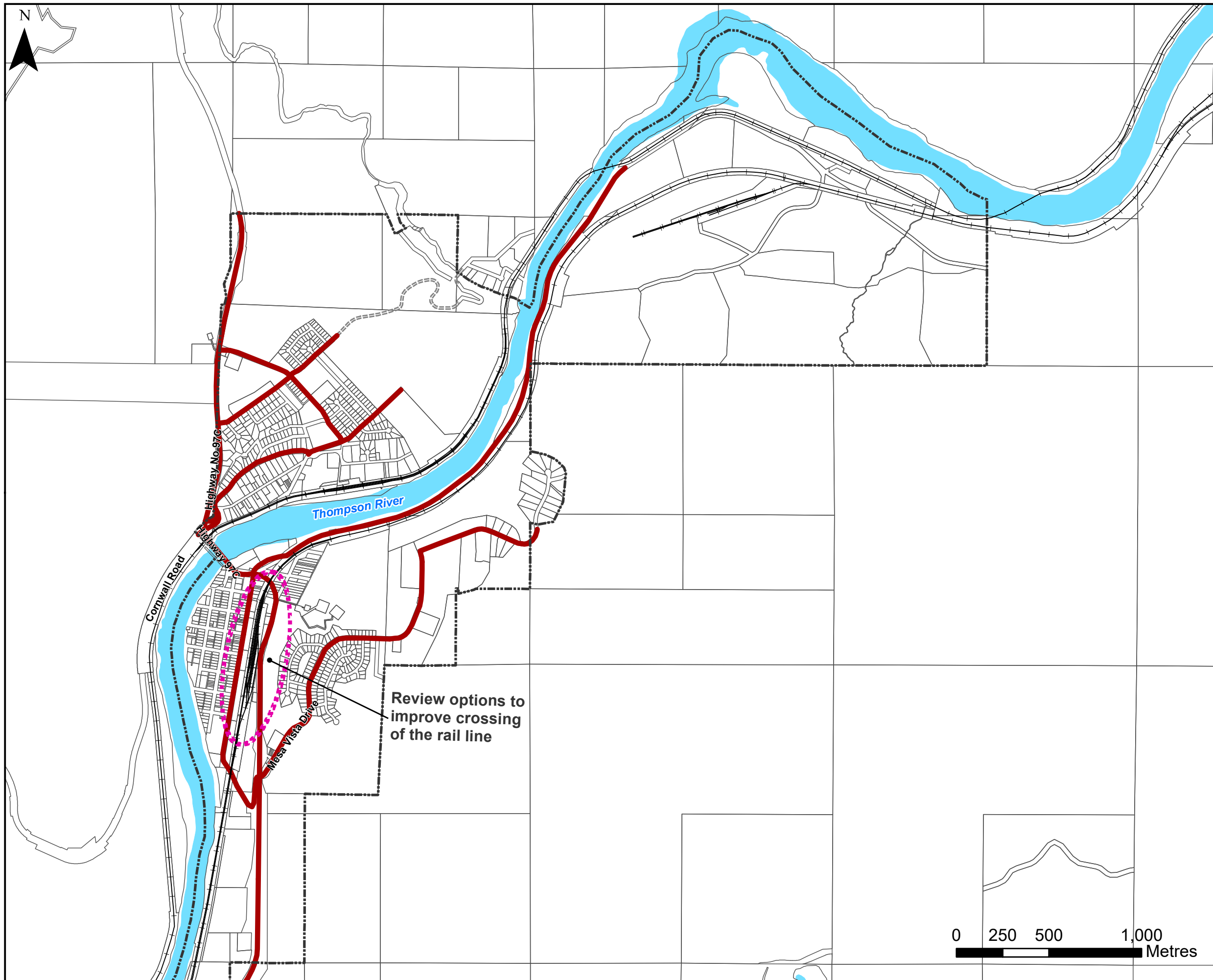
Water and road features obtained through www.geobase.ca website.

Cadastral information obtained through ParcelMap BC.
Municipal Boundary obtained through DataBC.

THE ACCURACY & COMPLETENESS OF INFORMATION SHOWN ON THIS DRAWING IS NOT GUARANTEED. IT WILL BE THE RESPONSIBILITY OF THE USER OF THE INFORMATION SHOWN ON THIS DRAWING TO LOCATE & ESTABLISH THE PRECISE LOCATION OF ALL EXISTING INFORMATION WHETHER SHOWN OR NOT.



Schedule F
Major Road Network



Village of Ashcroft

**Official Community Plan
Schedule F**

Major Road Network

Legend

 MAJOR ROAD

SOURCE:

Water and road features obtained through www.geobase.ca website.

Cadastral information obtained through ParcelMap BC.
Municipal Boundary obtained through DataBC.

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Schedule G

Downtown Façade Development Guidelines

Schedule G – Downtown Façade Design Guidelines

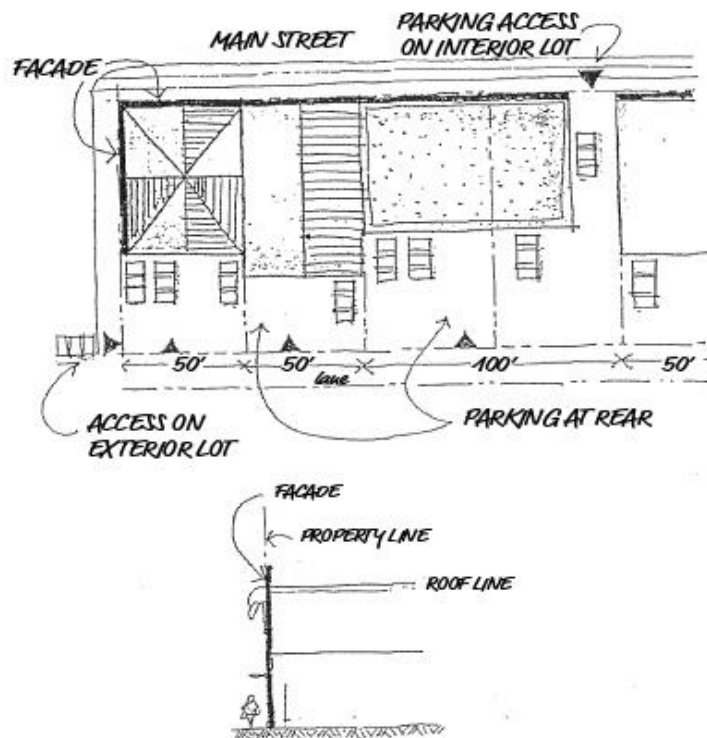
Guidelines:

Design guidelines, evoking the building patterns of turn-of-the-century Ashcroft, are intended to facilitate the revitalization of this commercial district. They cover the following:

- (1) Façade location;
- (2) Façade width;
- (3) Façade height;
- (4) Ground floor design;
- (5) Upper floor design;
- (6) Roof and roof edge details;
- (7) Canopies and porches;
- (8) Façade materials;
- (9) Façade colours;
- (10) Signage;
- (11) Lighting; and
- (12) Surface parking lots.

1. Façade Location

Except for automobile service stations, all building facades facing streets should be located at the street property lines. The façade is attached to and is part of the building.



Schedule G – Development Permit Area No. 4
Downtown Façade Design Guidelines

2. Façade Width

All building facades facing streets should be the full width of the building. On corner sites, the secondary (or side) façade may be less than the full depth of the property in order to accommodate parking and loading facilities at the rear of the building. On interior sites greater than 30 metres in width the front façade may be less than the full width of the property in order to accommodate access to parking at the rear of the building.

3. Façade Heights

Facades should be one or two storeys in height. For a one storey building, the cornice at the top of the parapet or false front, or the average rood height in the case of a gable roof without a false front, should be at least 4 metres above grade; for a two storey building, at least 4 metres tall, as well, the entire building should be keeping with the Development Permit Area No. 4.

Where a new building is built with more than two storeys, all storeys above the second should be set back from the property line at least 3 metres.

4. Ground Floor Design

Commercial store fronts should be designed in repetitive façade modules having widths of either one-half, one-third or one-quarter of the width of a standard lot. On Railway Avenue and Brink Street, a standard lot is 15.24 metres wide; thus the widths of the façade modules should be:

- Half width = 7.62 metres
- One third width = 5.08 metres
- One quarter width = 3.81 metres

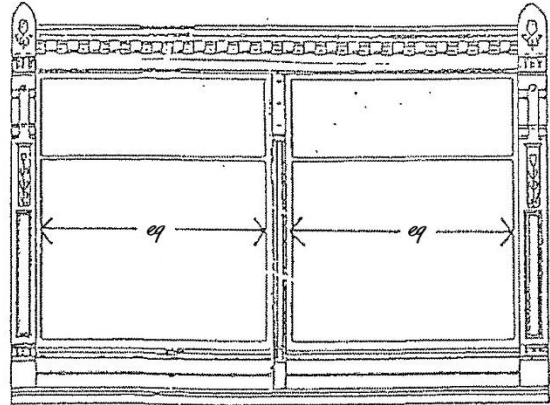
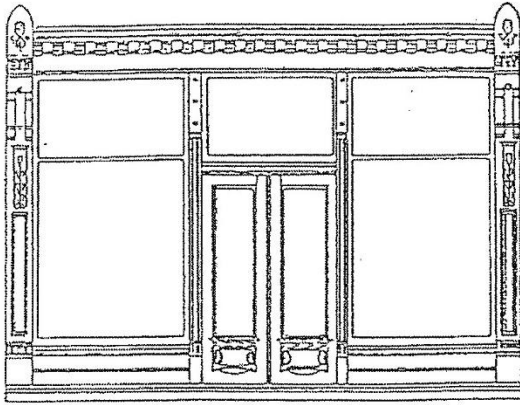
on any property comprised of one or more standard lots. Only one module width should be used on any single building. For any property whose width is not an even multiple of 15.24 metres, the façade modules should be equal in width and approximate the width noted above.

Storefronts should exhibit the following features:

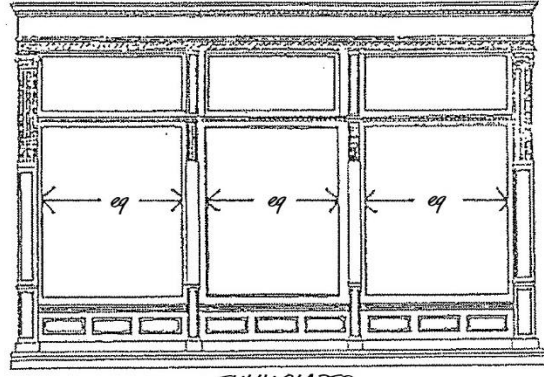
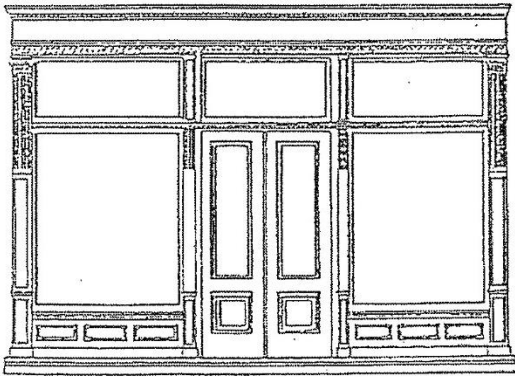
- a) Large display windows;
- b) Glazed entry doors;
- c) Window sills at 23 cm to 38 cm above grade; and
- d) Clear storey windows or opaque panels above the height of the door header.

Some examples are shown on the following pages:

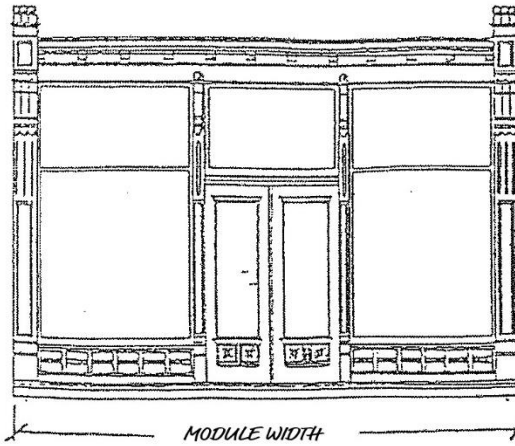
Schedule G – Development Permit Area No. 4
Downtown Façade Design Guidelines



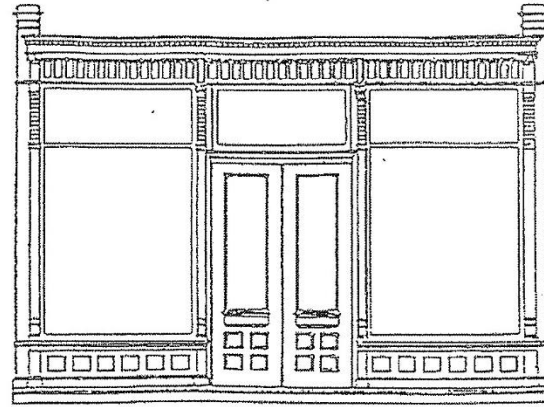
FULLY GLAZED



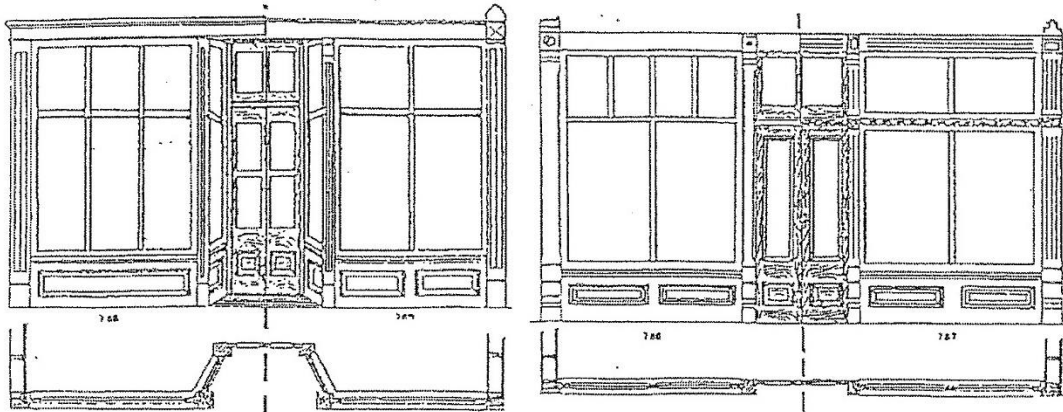
FULLY GLAZED



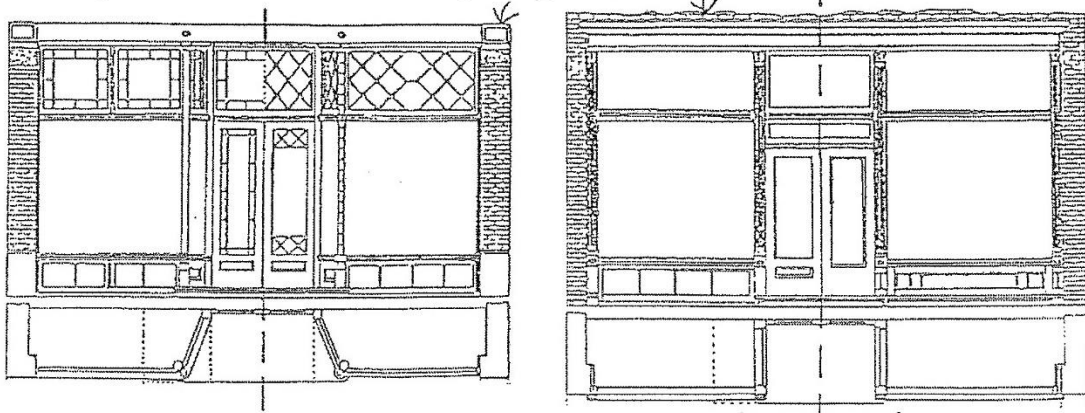
MODULE WIDTH



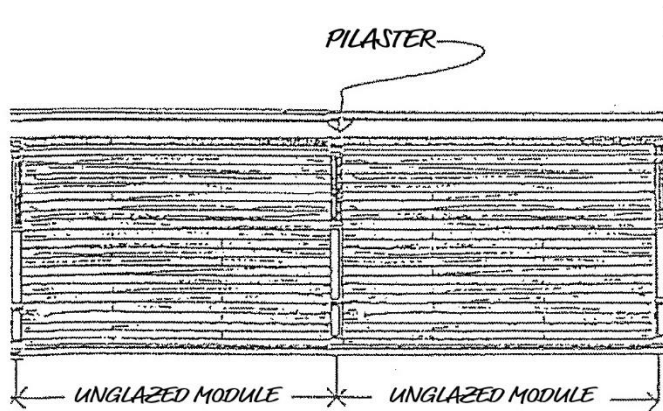
Schedule G – Development Permit Area No. 4
Downtown Façade Design Guidelines



MODULES WITH EXISTING BRICK COLUMNS

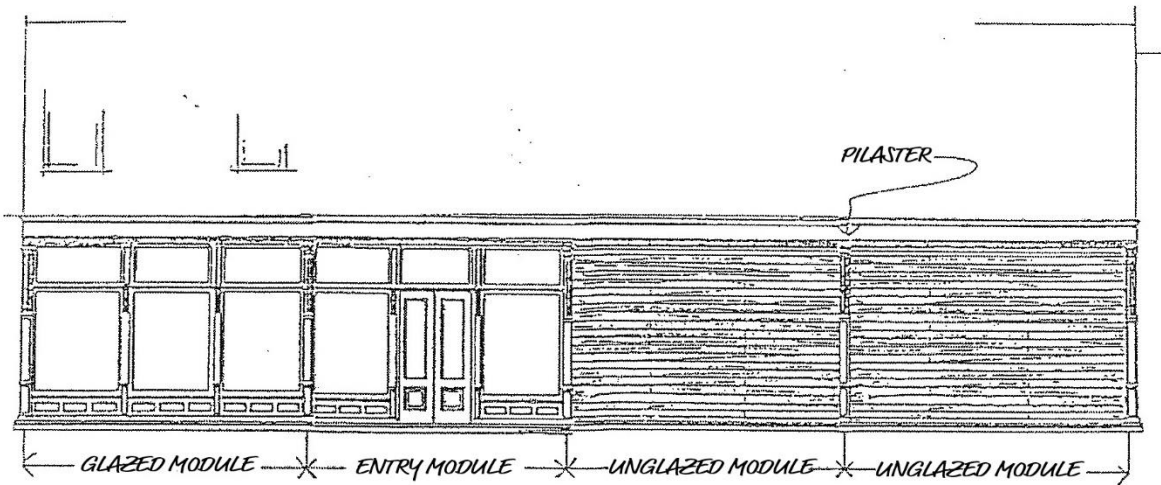


If the interior layout of the commercial premises does not permit glazing in every module, the unglazed modules should have no opening at all. Where several unglazed modules exist side by side, the module width should still be expressed by repeating the same edge pilaster used in the glazed modules.



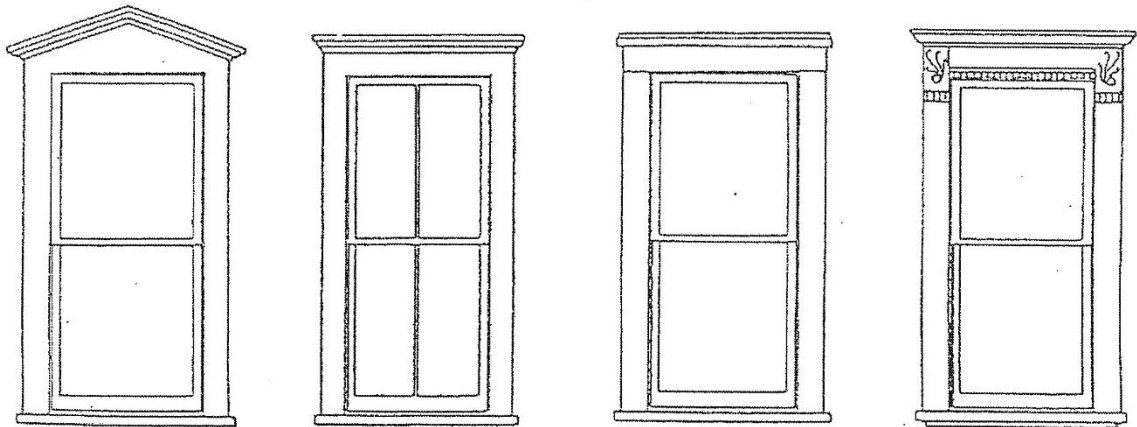
Schedule G – Development Permit Area No. 4
Downtown Façade Design Guidelines

Combining these various modules in a single building could result in the following:



5. Upper Floor Design

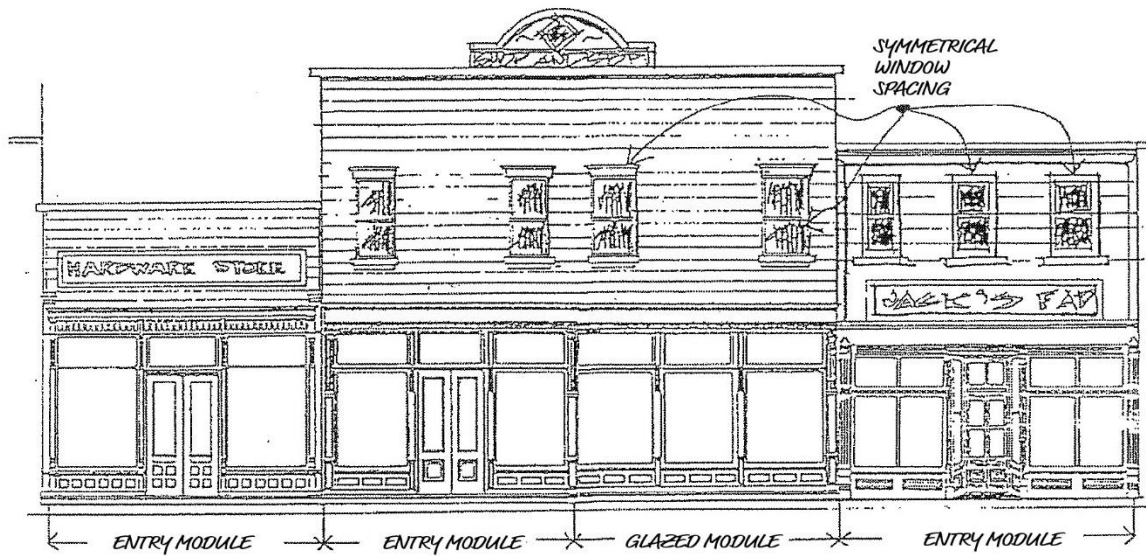
Upper floor windows should be double-hung with vertical dimensions roughly twice their horizontal dimensions and should have traditional sash and frame details. Some examples are:



The spacing of the upper floor windows should be related to the module width used on the ground floor and should be symmetrical within that module.

Window patterns above the second floor need not adhere to these guidelines as the wall in which they are located is set back at least 3.0 metres from the property line.

Schedule G – Development Permit Area No. 4
Downtown Façade Design Guidelines



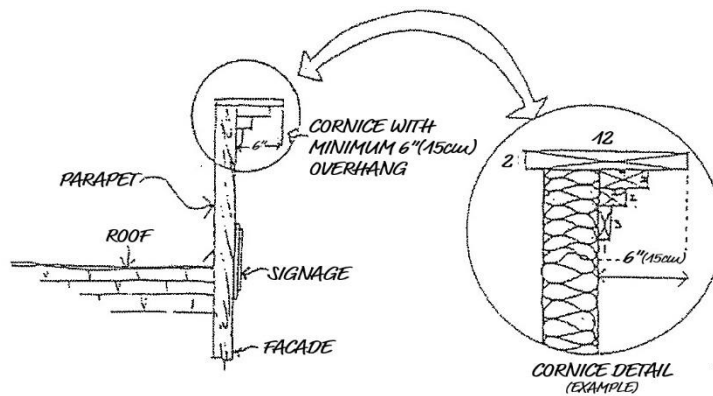
6. Roofs and Cornices

Buildings should have either flat roofs (i.e. with less than a 1 in 12 slope away from the front) or longitudinal gable roofs (i.e. with the ridge line perpendicular to the building frontage). On buildings with gable roofs, it is preferable (but not necessary) that the exposed gable be covered by a typical western false front capped with a cornice. Corner buildings with gable roofs should present an exposed gable to both street facades by means of a cruciform gable roof. Flat roofed buildings should have a parapet along the entire frontage capped with a cornice.

Schedule G – Development Permit Area No. 4
 Downtown Façade Design Guidelines

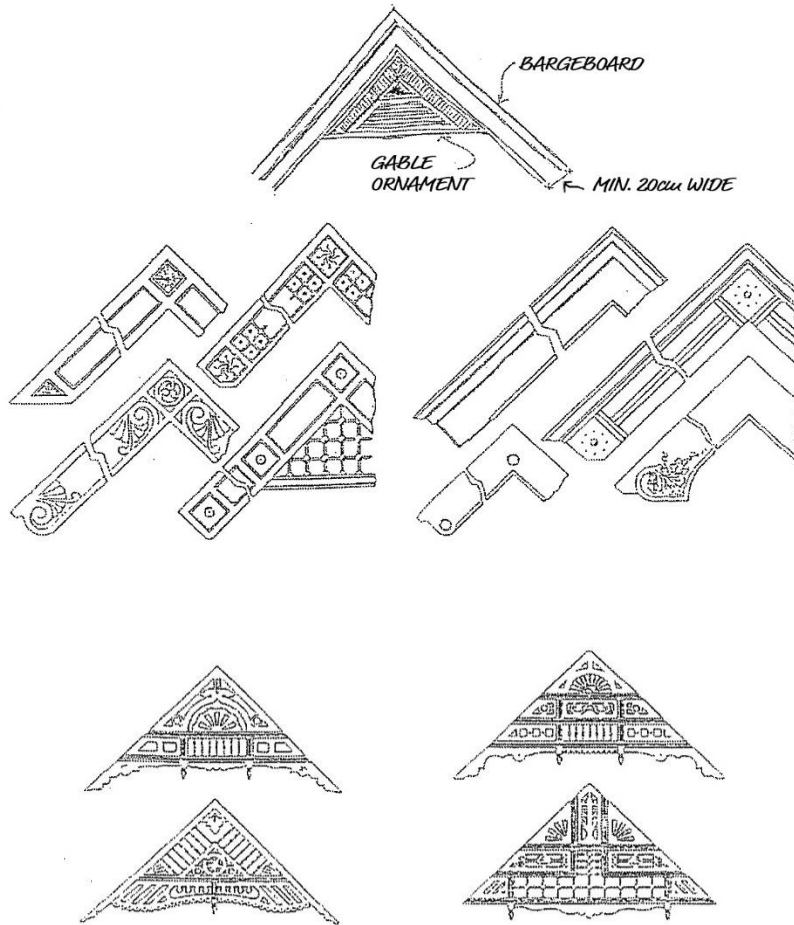


A cornice need not be elaborate but should protrude at least 10 cm from the façade in order to produce a discernable shadow line at the top of the wall. The sketch below is an example of such a cornice.



On gable roofed buildings, the fascia or bargeboard should be at least 20 cm deep and may be patterned as shown on the examples below.

Schedule G – Development Permit Area No. 4
Downtown Façade Design Guidelines



7. Canopies and Porches with Milled Wood Components.

Each building should have a canopy or porch extending over the sidewalk for the entire width of the façade. These structures should be at least 3 metres above the sidewalk and should project 2.5 metres from the façade or to within 0.6 metres of the curb whichever is less. For the purposes of these guidelines, these structures fall into two distinct types: canopies and porches.

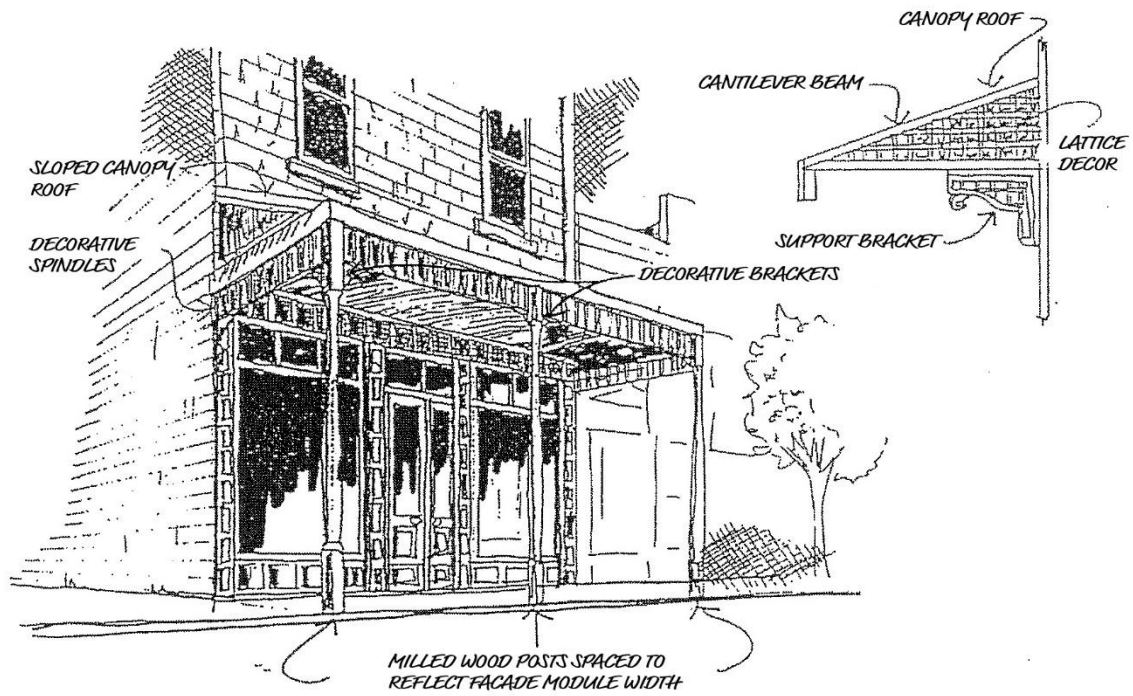
7.1 Canopies

Canopies are sloped downward as they project away from the building. The slope should be relatively shallow ranging from a 4 in 12 slope (15 degrees) to a 6 in 12 slope (22.5 degrees). Canopies may be supported either by posts or cantilever beams as shown below:

Posts should be spaced to reflect the façade module width.

Cantilever beams should be similarly spaced and should include a milled decorative support bracket as shown.

Schedule G – Development Permit Area No. 4
Downtown Façade Design Guidelines



7.2 Porches

Porches are flat roofed and are always supported by posts spaced as shown in the previous example. Porches also utilize decorative brackets at the top of the posts, balusters and handrails above the porch roof and sometimes spindles beneath the porch roof. The following sketch shows an example of typical porch construction with typical milled wood components.

Schedule G – Development Permit Area No. 4
Downtown Façade Design Guidelines

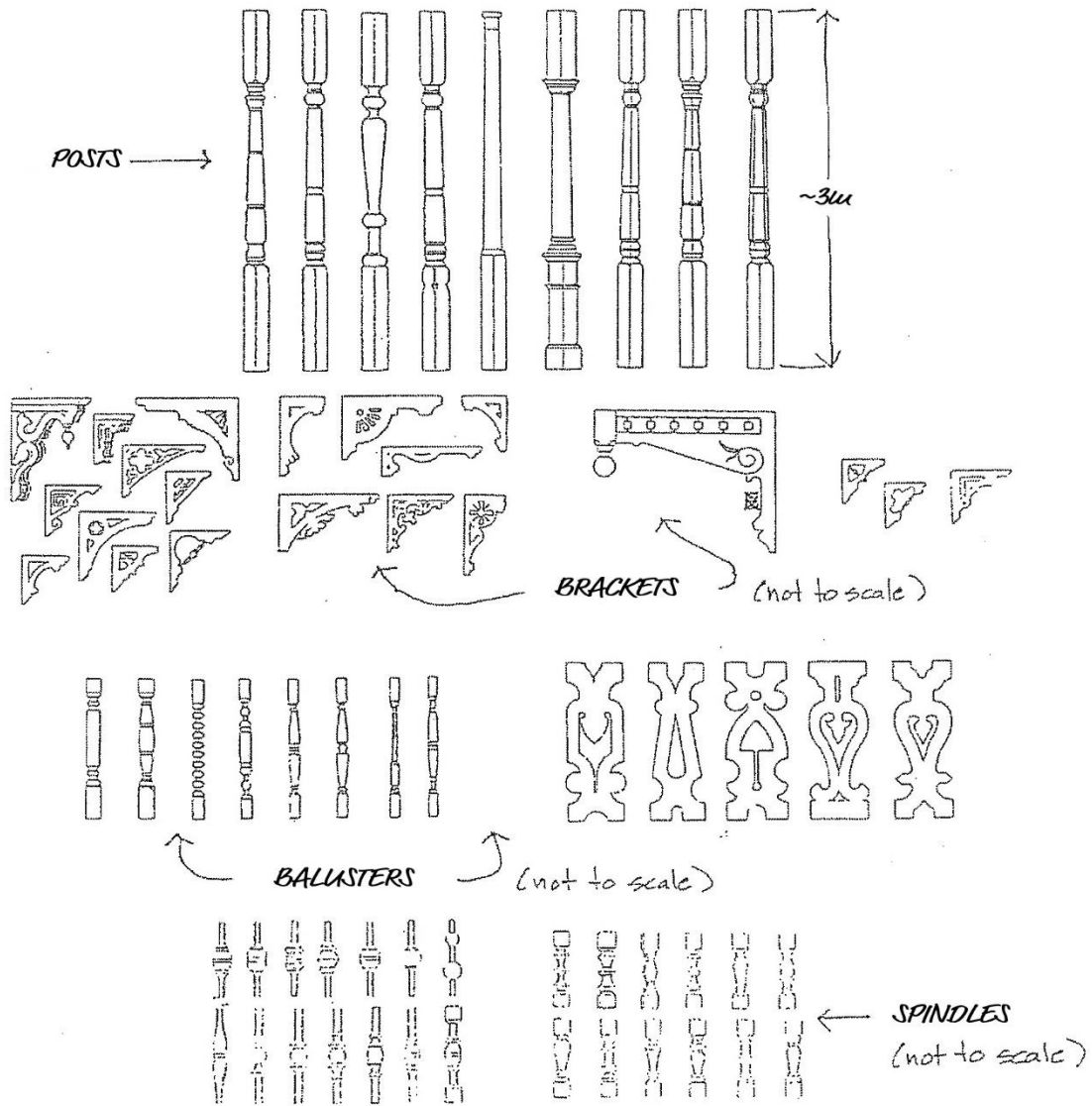


Historically, porches acted both as a roof over the sidewalk and as a balcony with access from the second floor of the adjacent building. Therefore, multi-storey buildings should have porches (with or without access) rather than canopies.

7.3 Milled Wood Components

Posts, brackets, balusters, handrails and spindles should reflect Victorian/Edwardian designs. Millwork of this era ranged from being relatively simple to being extremely ornate, but all was “manufactured” to some degree. The degree of elaborateness should be up to each developer noting, however, that the millwork typical of turn-of-the-century Ashcroft tended to be simple rather than overly ornate. The following are some examples of these milled elements:

Schedule G – Development Permit Area No. 4
Downtown Façade Design Guidelines



8. Façade Materials

Acceptable façade materials are:

- a) Glass;
- b) Wood siding and roofing;
- c) Milled wood detailing; and
- d) Other materials that accurately reflect the heritage motif

8.1 Glass

Glass used in doors and windows may be plain or tinted but should not be reflective. Glass that is coloured, stemmed, beveled or leaded or leaded may be appropriate in some storefront transoms.

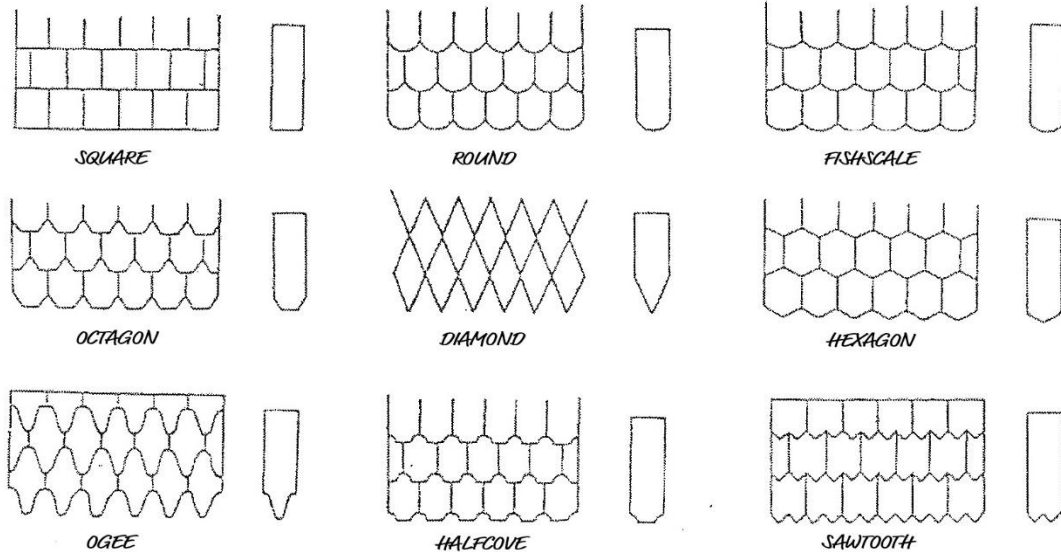
Schedule G – Development Permit Area No. 4
Downtown Façade Design Guidelines

8.2 Siding and Roofing

Façade siding should use materials that reflect the heritage motif and include either:

- a) Horizontal shiplap (which is preferred); or
- b) Shingles – which are used primarily to decorate gables, but can be used in lieu of shiplap. (split shakes are not an appropriate substitute for shingles.)

Below are some examples of shingle patterns:



8.3 Milled Wood Detailing

Wood detailing – Window and door sash and frames, bargeboards, brackets, posts, handrails, balusters, spindles and cornices – should be composed of materials that reflect the heritage motif and proper building code standards.

9. Façade Colours

Each store and/or building should be painted a different colour using appropriate heritage colours.

10. Signage

Signage shall be permitted according to the Village of Ashcroft's *Signage Bylaw*. Notwithstanding the *Signage Bylaw*, it is preferred that permanent signage be restricted to:

- a) Painted window signs;
- b) Painted signs that accurately reflect the heritage motif attached either to the upper façade or porch balusters; and
- c) Painted and projecting signs that accurately reflect the heritage motif (suspended under canopies and porches where such structures exist).

Backlit signs are to be discouraged. Signs lit from the front with warm lighting are encouraged.

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11. Lighting

Storefronts and sidewalks should be illuminated by warm coloured lighting attached to the underside of the canopies and porches. This should be seen as a supplement to existing street lighting.

12. Surface Parking Lots.

Generally, surface parking should be at the rear of the lot with access to that parking creating the minimum disruption to the continuous street wall. Where surface parking must front on the street, a 1 metre wide landscape strip containing street trees at roughly 7 metre centres and public benches should be installed and maintained. Surface parking lots shall be paved.

